

### PROPERTY OWNER CONSENT AND AUTHORIZATION

This consent is required for all Planning, Building, and Engineering applications. A completed copy must be included with every application, if the applicant is not the property owner.

If the Property Owner is a corporation/other legal entity, then the corporation/other legal entity should be listed as the owner. In such cases, proof of signature authority must be attached as well. If the Property is owned by multiple entities (as in a family trust, or otherwise co-owners), a signed consent form is required to be completed by each individual.

The Property Owner may designate a representative for the application review process.

- The Owner or Representative is solely responsible for the accuracy of the application documentation (electronic and paper forms/drawings/exhibits/narratives, etc.)
- The Owner or Representative is solely responsible for the accuracy and coordination of the submitted application documentation (electronic and paper forms/drawings/exhibits/narratives, etc.) to the County or other jurisdictions requiring permit approval.
- The Owner or Representative is solely responsible for scheduling any required inspections and approvals prior to the commencement of the use and/or the completion of improvements to the property.

### AUTHORIZATION BY PROPERTY OWNER(S)

I, \_\_\_\_\_, or the authorized representative(s) listed below, are submitting this Application, and am solely responsible for its accuracy and completeness. I understand that incomplete or inaccurate applications can lead to a delay in review and processing. I further confirm that I am the owner of the property at \_\_\_\_\_, PID(s) # \_\_\_\_\_, that is/are the subject of this application. I further confirm that I am fully aware of the County's application fee(s) and procedural requirements, and I hereby consent to this application.

\_\_\_\_\_  
Property Owner Signature

\_\_\_\_\_  
Date

### AUTHORIZED REPRESENTATIVE INFORMATION\*

I, Lori H. Schweller, Williams Mullen, confirm that I am the authorized representative for this Application and understand the requirements as noted above.

Lori H. Schweller  
Authorized Representative Signature

5/29/2026  
Date

*\*Only used when someone other than the property owner is submitting the application.*

*Industrious | Diverse | Steadfast | Helpful | Ethical*



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**AUTHORIZATION BY PROPERTY OWNER(S)**

I, J Eric Worderley, or the authorized representative(s) listed below, are submitting this Application, and am solely responsible for its accuracy and completeness. I understand that incomplete or inaccurate applications can lead to a delay in review and processing. I further confirm that I am the owner of the property at 4550 Dike Rd Prot VA 24188 (s) # 021247, that is/are the subject of this application. I further confirm that I am fully aware of the County's application fee(s) and procedural requirements, and I hereby consent to this application.

J Eric Worderley  
 Property Owner Signature

5-22-2026  
 Date

**AUTHORIZED REPRESENTATIVE INFORMATION\***

I, \_\_\_\_\_, confirm that I am the authorized representative for this Application and understand the requirements as noted above.

\_\_\_\_\_  
 Authorized Representative Signature

\_\_\_\_\_  
 Date

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- The Owner or Representative is solely responsible for scheduling any required inspections and approvals prior to the commencement of the use and/or the completion of improvements to the property.

**AUTHORIZATION BY PROPERTY OWNER(S)**

I, Jean M. Wonderley, or the authorized representative(s) listed below, are submitting this Application, and am solely responsible for its accuracy and completeness. I understand that incomplete or inaccurate applications can lead to a delay in review and processing. I further confirm that I am the owner of the property at 4150 Old Pike Road, Pilot, Virginia, PID(s) # 021247, that is/are the subject of this application. I further confirm that I am fully aware of the County's application fee(s) and procedural requirements, and I hereby consent to this application.

Jean M. Wonderley  
 Property Owner Signature

May 22, 2026  
 Date

**AUTHORIZED REPRESENTATIVE INFORMATION\***

I, \_\_\_\_\_, confirm that I am the authorized representative for this Application and understand the requirements as noted above.

\_\_\_\_\_  
 Authorized Representative Signature

\_\_\_\_\_  
 Date

*\*Only used when someone other than the property owner is submitting the application.*

*Industrious | Diligent | Steadfast | Helpful | Ethical*



DEVELOPMENT REVIEW TEAM  
PLANNING | PSA | ENGINEERING  
755 Roanoke Street, Suite 2A  
Christiansburg, VA 24073

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- The Owner or Representative is solely responsible for scheduling any required inspections and approvals prior to the commencement of the use and/or the completion of improvements to the property.

### AUTHORIZATION BY PROPERTY OWNER(S)

I, Lisa Ellen Wonderley Forster, or the authorized representative(s) listed below, are submitting this Application, and am solely responsible for its accuracy and completeness. I understand that incomplete or inaccurate applications can lead to a delay in review and processing. I further confirm that I am the owner of the property at 4750 Old Pike Road, Pilot, Virginia PID(s) # 021247, that is/are the subject of this application. I further confirm that I am fully aware of the County's application fee(s) and procedural requirements, and I hereby consent to this application.

Lisa W. Forster  
Property Owner Signature

5-25-2026  
Date

### AUTHORIZED REPRESENTATIVE INFORMATION\*

I, \_\_\_\_\_, confirm that I am the authorized representative for this Application and understand the requirements as noted above.

\_\_\_\_\_  
Authorized Representative Signature

\_\_\_\_\_  
Date

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*Industrious | Diverse | Steadfast | Helpful | Ethical*



April 28, 2026

Montgomery County Planning  
755 Roanoke Street  
Suite 2A  
Christiansburg, VA 24073  
Attn: Zoning Administrator

RE: US-VA-5456 Old Pike Communications Facility – Maintenance and Authorization Letter

Dear Zoning Administrator:

In accordance with the Montgomery County Zoning Ordinance Sec. 10-48(6)(d)-(f), The Towers, LLC and any successor in interest commits that it will:

- a. Respond within a commercially reasonable time period to a request for information from a potential shared use applicant;
- b. Negotiate in good faith concerning future requests for shared use of the new tower by other telecommunications providers; and
- c. Allow shared use of the new Telecommunications Tower if another telecommunications provider agrees in writing to pay reasonable charges. The charge may include but is not limited to a pro rata share of the cost of site selection, planning, project administration, land costs, site design, construction and maintenance financing, return on equity, depreciation, and all the costs of adapting the tower or equipment to accommodate a shared user without causing electromagnetic interference.

With respect to the above application, The Towers, LLC (or the then-current communications facility owner) agrees to remove its facilities at the site, at its cost and expense, if the communications facility becomes obsolete or ceases to be used for its intended purpose by all existing collocators for a period of three hundred and sixty-five (365) consecutive days.

The proposed Telecommunications Facility shall comply with all Federal Aviation Administration (FAA) requirements, including those relating to the Virginia Tech Airport.

Thank you for considering our application.

Sincerely,

Shannon Dupra  
Project Manager



# Antenna Structure Registration

[FCC](#) > [WTB](#) > [ASR](#) > [Online Systems](#) > TOWAIR

[FCC Site Map](#)

## TOWAIR Determination Results

[? HELP](#)

[New Search](#) [Printable Page](#)

### \*\*\* NOTICE \*\*\*

TOWAIR's findings are not definitive or binding, and we cannot guarantee that the data in TOWAIR are fully current and accurate. In some instances, TOWAIR may yield results that differ from application of the criteria set out in 47 C.F.R. Section 17.7 and 14 C.F.R. Section 77.13. A positive finding by TOWAIR recommending notification should be given considerable weight. On the other hand, a finding by TOWAIR recommending either for or against notification is not conclusive. It is the responsibility of each ASR participant to exercise due diligence to determine if it must coordinate its structure with the FAA. TOWAIR is only one tool designed to assist ASR participants in exercising this due diligence, and further investigation may be necessary to determine if FAA coordination is appropriate.

#### DETERMINATION Results

**Structure does not require registration. There are no airports within 8 kilometers (5 miles) of the coordinates you provided.**

#### Your Specifications

##### NAD83 Coordinates

Latitude	37-06-18.4 north
Longitude	080-47-30.5 west

##### Measurements (Meters)

Overall Structure Height (AGL)	45.7
Support Structure Height (AGL)	44.2
Site Elevation (AMSL)	675.4

##### Structure Type

MTOWER - Monopole

#### [Tower Construction Notifications](#)

Notify Tribes and Historic Preservation Officers of your plans to build a tower.

#### ASR Help

[FAQ](#) - [Online Help](#) - [Documentation](#) - [Technical Support](#)

#### ASR Online Systems

[TOWAIR](#)- [CORES](#) - [ASR Online Filing](#) - [Application Search](#) - [Registration Search](#)

#### About ASR

[Privacy Statement](#) - [About ASR](#) - [ASR Home](#)

\*\*\*\*\*  
\* Federal Airways & Airspace \*  
\* Summary Report: New Construction \*  
\* Antenna Structure \*  
\*\*\*\*\*

Airspace User: VB

File: US-VA-5092

Location: Pulaski, VA

Latitude: 37°-06'-18.37" Longitude: 80°-47'-30.46"

SITE ELEVATION AMSL.....2216 ft.

STRUCTURE HEIGHT.....150 ft.

OVERALL HEIGHT AMSL.....2366 ft.

#### NOTICE CRITERIA

- FAR 77.9(a): NNR (DNE 200 ft AGL)
- FAR 77.9(b): NNR (DNE Notice Slope)
- FAR 77.9(c): NNR (Not a Traverse Way)
- FAR 77.9: NNR FAR 77.9 IFR Notice for PSK
- FAR 77.9: NNR (No Expected TERPS® impact with BCB)
- FAR 77.9(d): NNR (Off Airport Construction)

NR = Notice Required

NNR = Notice Not Required

PNR = Possible Notice Required (depends upon actual IFR procedure)  
For new construction review Air Navigation Facilities at bottom  
of this report.

Notice to the FAA is not required at the analyzed location and height for  
slope, height or Straight-In procedures. Please review the 'Air Navigation'  
section for notice requirements for offset IFR procedures and EMI.

#### OBSTRUCTION STANDARDS

- FAR 77.17(a)(1): DNE 499 ft AGL
- FAR 77.17(a)(2): DNE - Airport Surface
- FAR 77.19(a): DNE - Horizontal Surface
- FAR 77.19(b): DNE - Conical Surface
- FAR 77.19(c): DNE - Primary Surface
- FAR 77.19(d): DNE - Approach Surface
- FAR 77.19(e): DNE - Approach Transitional Surface
- FAR 77.19(e): DNE - Abeam Transitional Surface

VFR TRAFFIC PATTERN AIRSPACE FOR: PSK: NEW RIVER VALLEY

Type: A RD: 32134.36 RE: 2104.6

FAR 77.17(a)(1): DNE

FAR 77.17(a)(2): DNE - Height No Greater Than 200 feet AGL.  
 VFR Horizontal Surface: DNE  
 VFR Conical Surface: DNE  
 VFR Primary Surface: DNE  
 VFR Approach Surface: DNE  
 VFR Transitional Surface: DNE

VFR TRAFFIC PATTERN AIRSPACE FOR: BCB: VIRGINIA TECH/MONTGOMERY EXE  
 Type: A RD: 115099.4 RE: 2119.2

FAR 77.17(a)(1): DNE  
 FAR 77.17(a)(2): DNE - Greater Than 5.99 NM.  
 VFR Horizontal Surface: DNE  
 VFR Conical Surface: DNE  
 VFR Primary Surface: DNE  
 VFR Approach Surface: DNE  
 VFR Transitional Surface: DNE

TERPS DEPARTURE PROCEDURE (FAA Order 8260.3, Volume 4)  
 FAR 77.17(a)(3) Departure Surface Criteria (40:1)  
 DNE Departure Surface

MINIMUM OBSTACLE CLEARANCE ALTITUDE (MOCA)  
 FAR 77.17(a)(4) MOCA Altitude Enroute Criteria  
 The Maximum Height Permitted is 3500 ft AMSL

PRIVATE LANDING FACILITIES  
 No Private Landing Facilities Are Within 6 NM

AIR NAVIGATION ELECTRONIC FACILITIES

	FAC		ST		DIST	DELTA		GRND	
APCH	IDNT	TYPE	AT	FREQ	VECTOR	(ft)	ELEVA ST	LOCATION	ANGLE
BEAR	-----								
	PSK	VORTAC	R	116.8	105.44	23881	+239	VA PULASKI	.57
	No Impact. Does Not Exceed Navaid EMI Notice Height Criteria.								
	BLF	VOR/DME	R	115.0	302.14	138246	-534	WV BLUEFIELD	-.22
	FCX	RADAR WXL	Y	5250.	101.16	153853	-599	VA ROANOKE-BLACKSBUR	-.22

C-BAND 3.7-3.98 GHz COORDINATION ZONE  
 No Identified 5G conflict.

CFR Title 47, §1.30000-§1.30004  
 AM STUDY NOT REQUIRED: Structure is not near a FCC licensed AM station.  
 Movement Method Proof as specified in §73.151(c) is not required.

Please review 'AM Station Report' for details.

Nearest AM Station: WBLB @ 4414 meters.

Airspace® Summary Version 26.1.716

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03-13-2026

13:24:36

## **The Towers, LLC — Site Name: Old Pike**

### **SPECIAL USE PERMIT – APPLICANT NARRATIVE**

#### **Request**

The Towers, LLC (the "Applicant"), a joint venture of Vertical Bridge and Verizon Wireless, proposes to construct and operate a 199-foot self-support lattice telecommunications tower with supporting ground equipment within a 100' x 100' lease area on a 124.24-acre parcel located at 4752 Old Pike Road, Pilot, VA 24138 ("Property"). The Property is owned by John E. Wonderley and Lisa E. Wonderley Forster with a life estate held by Jean M. Wonderley. Vertical Bridge will lease the Property from the Wonderleys and construct and manage the communications facility. Verizon Wireless ("VZW") has a need for improved coverage in this area and will be the anchor tenant on the tower.

#### **Design and Compatibility**

The proposed tower will have an overall structure height of 199 feet (195-foot tower with a 4-foot lightning rod), which will accommodate VZW's antennas and at least three (3) additional future collocators. The overall lease area will be 100' x 100' (approximately 10,000 square feet) and the fenced compound will be 50' x 50'. The tower and associated ground equipment will be compatible with the surrounding area for the following reasons:

- The tower is strategically sited so as to blend in with the natural surroundings of the A-1, Agricultural District and surrounding properties;
- The tower will be approximately 156 feet, 8 inches from Old Pike Road (State Route 615), far exceeding the A-1 40-foot front setback and the 100' setback for structures over forty feet tall;
- The tower will be approximately 433 feet from the closest off-site dwelling;
- All structural elements of the tower shall be hot-dipped galvanized steel to match the backdrop of the sky, thereby reducing visibility;
- The ground equipment will be enclosed by a 6-foot chain-link security fence topped with 1 foot of barbed wire;
- The facility will be unmanned and will only be visited approximately two times per month by technicians for periodic maintenance and inspection;
- The facility will not emit any significant noise, smoke, dust, or odor;
- The facility is not intended for human habitation and does not require potable water or sanitary service.

#### **Purpose of Tower**

In today's society, reliable wireless service is a necessity for residents, students, businesses, visitors, and emergency personnel. The proposed tower will provide the infrastructure needed for VZW, as well as multiple other wireless carriers through future collocation, to expand and improve their networks in this area of Montgomery County. The tower has been designed to accommodate antenna centerlines for VZW at the 192-foot level, with future carrier positions at approximately 180 feet, 170 feet, and 160 feet above ground level. This reliable wireless service will enhance and protect the local economy and complement economic development by (1) providing citizens the opportunity to work and shop from home, (2) allowing students the ability to utilize the latest online educational opportunities while at home, (3) helping nearby businesses to be more effective and efficient in their everyday operations, and (4) providing dependable wireless service to citizens travelling and emergency services operating on roadways in the area. This tower will specifically provide the latest wireless technologies to those living in the greater Pilot area and surrounding rural communities, those travelling on Old Pike Road, State Route 615, and surrounding neighborhood roads, and emergency personnel operating in the area.

**Ordinance Requirements**

The subject property is zoned A-1, Agricultural. In accordance with the Montgomery County Zoning Ordinance Sec. 10-21(4)(ay), telecommunications towers are permitted in the A-1, Agricultural District with an approved Special Use Permit ("SUP"). Wireless Telecommunication Facilities are governed within Section 10-48, Additional Regulations for Special Uses, of the Zoning Ordinance. The applicable requirements in this section are as follows:

*The tower will far exceed all required setbacks, including the primary structure setbacks for the A-1, Agricultural District. The following are the required and proposed setbacks:*

<b>Setback</b>	<b>Required</b>	<b>Proposed</b>
Front	40'* / 100'	156'-8" ±
Side	15'* / 100'	414'-8" ±
Rear	40'* / 100'	4,098'-1" ±

For every one (1) foot above forty (40) feet, the building or structure shall be set back an additional two (2) feet up to a maximum of one hundred (100) feet.

**§ 10-48. Additional Regulations for Special Uses.**

**6. Telecommunications towers, freestanding.**

**(a)** Such towers shall be maintained with a galvanized steel finish or be painted a neutral color.

*All structural elements of the proposed tower will be hot-dipped galvanized steel.*

**(b)** Dish antennas shall be a neutral, nonreflective color.

*Duly noted.*

**(c)** No logos or advertising of any kind shall be permitted on towers, antennas, or any accompanying structures or facilities.

*The only signage that is proposed is required by the FCC and will include a site identification sign on the compound gate.*

**(d)** A written agreement for permitting future co-locations of telecommunications facilities shall be provided by the tower owner and maintained to the satisfaction of the zoning administrator.

*The Towers, LLC / Vertical Bridge is open to future collocations and has enclosed with the SUP application a letter, dated April 28, 2026, from Shannon Dupra, Project Manager, providing details of the Applicant's commitment to collocations.*

**(e)** A written agreement assuring prompt removal of the tower upon abandonment, at the responsibility and cost of the tower owner or landowner, shall be provided and maintained to the satisfaction of the zoning administrator.

*The letter, dated April 28, 2026, from Shannon Dupra, Project Manager, confirms the Applicant's agreement to remove the facility if it becomes obsolete or is unused by all existing collocators for 365 consecutive days.*

(f) All towers shall comply with all Federal Aviation Administration (FAA) requirements, including those relating to the Virginia Tech Airport.

*See attached TOWAIR report. The proposed tower is not within eight kilometers of any airports.*

### **§ 10-54.3. Special Use Permits**

g. Issues for consideration. In considering a special use permit application, the following factors shall be given reasonable consideration. The applicant shall address all the following in its statement of justification or concept development plan unless not applicable, in addition to any other standards imposed by this chapter:

#### **1. Whether the proposed special use permit is consistent with the comprehensive plan.**

*The proposed wireless facility is consistent with the goals of the Montgomery County Comprehensive Plan ("Montgomery County 2025"). The subject parcel is located within the Pilot community, which is identified in the Comprehensive Plan as one of Montgomery County's eighteen designated "Rural Communities," small-scale, stable rural residential communities of local historic significance that have traditionally functioned as community focal points. Under PLU 1.4, the Comprehensive Plan provides that small-scale civic, institutional, and employment uses may be permitted in Rural Communities in locations that enhance the compact nature of these communities, provided they do not pose a threat to public health, safety, or welfare, and provided they are compatible with adjacent land uses. The proposed telecommunications facility is compatible with adjacent agricultural land uses, poses no threat to public health, safety, or welfare, and will serve the broader Pilot community and surrounding rural areas.*

*The Utilities chapter of the Comprehensive Plan establishes the goal to "[p]rovide for the orderly extension of electric service, telecommunication service (land line, wireless and/or cable) and natural gas service in a manner that supports growth and development without negatively impacting the natural environment" (UTL 2.0). This proposed site will extend wireless service into this area of Montgomery County to support economic development, educational opportunities, emergency communications, and overall coverage to the citizens of the County, thereby meeting this objective.*

*The Comprehensive Plan also supports co-location on existing structures under UTL 2.2.1, which encourages the siting of new antennae on existing structures such as communication towers, tall buildings, water tanks, electric transmission towers, and signs. VZW looked for a tower, building, or other tall structure within the search ring on which to collocate its antennas, and there were no towers, buildings, or transmission towers within the search ring that were tall enough, structurally capable, or properly spaced from surrounding existing sites to meet the coverage objective. The proposed tower therefore allows Montgomery County to accomplish the Comprehensive Plan's goal of encouraging co-location by providing a new structure capable of accommodating multiple carriers in an area where no viable co-location options currently exist.*

#### **2. Whether the proposed special use permit will adequately provide for safety from fire hazards and have effective measures of fire control.**

*The proposed tower will have a four-foot (4') lightning rod and will be grounded per electric power regulations. The site will be designed with a turnaround area allowing access to emergency vehicles.*

#### **3. The level and impact of any noise emanating from the site, including that generated by the proposed use, in relation to the uses in the immediate area.**

*No smoke, dust, odor, or significant noise will result from the proposed facility. There are no residential structures located immediately adjacent to the site, and the closest dwelling is approximately 433 feet away.*

#### **4. The glare or light that may be generated by the proposed use in relation to uses in the immediate area.**

*The proposed tower tower is not expected to require lighting per FAA standards regarding height. A 500-watt metal halide flood light on a timed switch is proposed within the compound for maintenance access purposes only.*

**5. The proposed location, lighting, and type of signs in relation to the proposed use, uses in the area, and the sign requirements of this chapter.**

*The only proposed signage is that required by the FCC and Vertical Bridge operational requirements, including a site identification sign, caution RF sign, and notice RF sign.*

**6. The compatibility of the proposed use with other existing or proposed uses in the neighborhood and adjacent parcels.**

*The proposed facility will provide wireless coverage and service to the Pilot community, surrounding agricultural and residential properties, and citizens and visitors travelling on nearby roadways. All adjacent properties are zoned A-1, Agricultural, and consist of large rural and agricultural parcels. The proposed tower is strategically sited to conform to the existing agricultural and rural surroundings of the area while effectively enhancing the coverage objectives of VZW and any future carriers.*

**7. The location and area footprint with dimensions (all drawn to scale), nature and height of existing or proposed buildings, structures, walls, and fences on the site and in the neighborhood.**

*As shown on the submitted zoning drawings, the proposed tower would be enclosed in a 50' x 50' fenced compound within a 100' x 100' lease area. The 195-foot tower (with a 4-foot lightning rod) and associated ground equipment, including VZW's integrated equipment and battery cabinet and generator pad, would be located entirely within the compound. The compound will be enclosed by a 6-foot chain-link fence topped with 1 foot of barbed wire to screen equipment and provide security.*

**8. The nature and extent of existing or proposed landscaping, screening, and buffering on the site and in the neighborhood.**

*The compound will be screened by existing tree lines and wooded areas that surround the proposed site, as depicted on the submitted zoning drawings. The 7-foot chain-link fence with barbed wire topper will further screen the ground equipment within the compound from view. Existing vegetation outside the lease area and access easement will be preserved to the greatest extent practicable.*

**9. The timing and phasing of the proposed development and the duration of the proposed use.**

*Construction shall commence once the zoning, site plan, and related permitting processes are complete, and all federal approvals have been obtained. The time that it takes to construct the facility is approximately three to four months after building permit approval. The facility is intended to operate as a long-term wireless telecommunications installation.*

**10. Whether the proposed special use permit will result in the preservation or destruction, loss or damage of any topographic or physical, natural, scenic, archaeological or historic feature of significant importance.**

*Vertical Bridge intends to utilize a proposed 12-foot-wide gravel access drive connecting to Old Pike Road, minimizing additional disturbance to the parcel. The compound will be graded and surfaced with gravel during the construction process. Existing trees and other shrubs will be removed only inside the proposed lease area and access/utility easement as needed.*

**11. Whether the proposed special use permit at the specified location will contribute to or promote the welfare or convenience of the public.**

*The proposed use will improve VZW's voice, data, and internet services in the area, which will benefit the welfare and convenience of the public by providing better communications for emergencies and better internet connectivity for work, school, and entertainment.*

**12. The traffic expected to be generated by the proposed use.**

*The proposed site will only be serviced by technicians approximately two times per month, or following an outage, after construction is completed. Therefore, the site will not generate any meaningful additional traffic.*

**13. Whether, in the case of existing structures proposed to be converted to uses requiring a special use permit, the structures meet all code requirements of Montgomery County.**

*Not applicable.*

**14. Whether the proposed special use permit will be served adequately by essential public facilities and services.**

*Not applicable. The facility is unmanned and does not require potable water or sanitary service.*

**15. The effect of the proposed special use permit on groundwater supply.**

*Not applicable.*

**16. The effect of the proposed special use permit on the structural capacity of the soils.**

*Not applicable.*

**17. Whether the proposed use will facilitate orderly and safe road development and transportation.**

*The proposed use will not have any effect on road development and transportation.*

**18. The effect of the proposed special use permit on environmentally sensitive land or natural features, wildlife habitat and vegetation, water quality and air quality.**

*The proposed use will not have any significant effect on natural resources, wildlife, or water and air quality. No significant noise, smoke, dust, or odor will result from the facility.*

**19. Whether the proposed special use permit use will provide desirable employment and enlarge the tax base by encouraging economic development activities consistent with the comprehensive plan.**

*Wireless service and coverage is essential to economic activity. Areas without adequate service are at a disadvantage in attracting businesses and new residents to the area. Now more than ever, employees and contractors are working from home or in the field and need good internet and cell phone connectivity. The proposed wireless facility will provide residents and students in the area the opportunity to work, attend classes, and take courses online.*

**20. Whether the proposed special use permit considers the needs of agriculture, industry, and businesses in future growth.**

*As noted above, wireless service and coverage are important to agriculture, industry, and business to provide necessary communications and internet access. The surrounding A-1 zoned properties are primarily agricultural in nature and will benefit from improved wireless connectivity.*

**21. The effect of the proposed special use permit use in enhancing affordable shelter opportunities for residents of the county.**

*Not applicable.*

**22. The provision for stormwater management and positive drainage within and adjacent to the development.**

*The proposed facility will cause only a "de minimis" increase in stormwater runoff; therefore, no drainage structures are proposed. Swales and/or drainage ditches will be utilized for proper water runoff as needed.*

**23. The location, character, and size of any outdoor storage.**

*Not applicable.*

**24. The proposed use of open space.**

*Not applicable.*

**25. The location of any major floodplain and steep slopes.**

*The site will not be located within or impact any floodplain or steep slope areas.*

**26. The location and use of any existing nonconforming uses and structures.**

*Not applicable.*

**27. The location and type of any fuel and fuel storage.**

*VZW proposes the installation of a generator on a 3.5' x 8' concrete pad within the compound, which may include a small fuel tank for diesel fuel to power the generator in the case of an electric outage.*

**28. The location and use of any anticipated accessory uses and structures.**

*Not applicable.*

**29. The area of each use, if appropriate.**

*As shown on the submitted zoning drawings, the tower and ground equipment will be located within a 50' x 50' fenced compound within a 100' x 100' lease area.*

**30. The proposed days/hours of operation.**

*The proposed use will operate 24 hours a day but is unmanned.*

**31. The location and screening of parking and loading spaces and/or areas.**

*As shown on the submitted zoning drawings, the site will utilize a proposed 12-foot-wide gravel access drive and turnaround area for means of parking and turnaround.*

**32. The location and nature of any proposed security features and provisions.**

*The compound will be entirely enclosed by a 6-foot chain-link fence topped with 1 foot of barbed wire for security, with anti-theft tension bars and a locked double-swing gate. A high-security combination padlock (ABUS 190CS) will be installed on the compound gate.*

**33. The number of employees.**

*The proposed facility is an unmanned facility and does not require any employees.*

**34. The location of any existing and/or proposed adequate on- and off-site infrastructure.**

*Power will be provided by Appalachian Power, and telecommunications service will be provided by Verizon. Power to the facility will be monitored by a separate meter. An 8-foot utility H-frame is proposed for electrical and fiber service connections.*

**35. Any anticipated odors which may be generated by the uses on site.**

*The proposed facility will not emit any odors.*

**36. Whether the proposed special use permit uses sufficient measure to mitigate the impact of construction traffic on existing neighborhoods and school areas.**

*As part of the permitting phase, Vertical Bridge and its contractor will coordinate with VDOT regarding any sight distances and flaggers for entering/exiting the site during construction. The contractor shall minimize disturbance to the existing site during construction, and erosion control measures shall be implemented in conformance with local guidelines for erosion and sediment control.*



Know what's below.  
Call before you dig.



## SITE NAME: OLD PIKE SITE NUMBER: US-VA-5456

4752 OLD PIKE ROAD  
PILOT, VA 24138  
MONTGOMERY COUNTY

### SITE INFORMATION

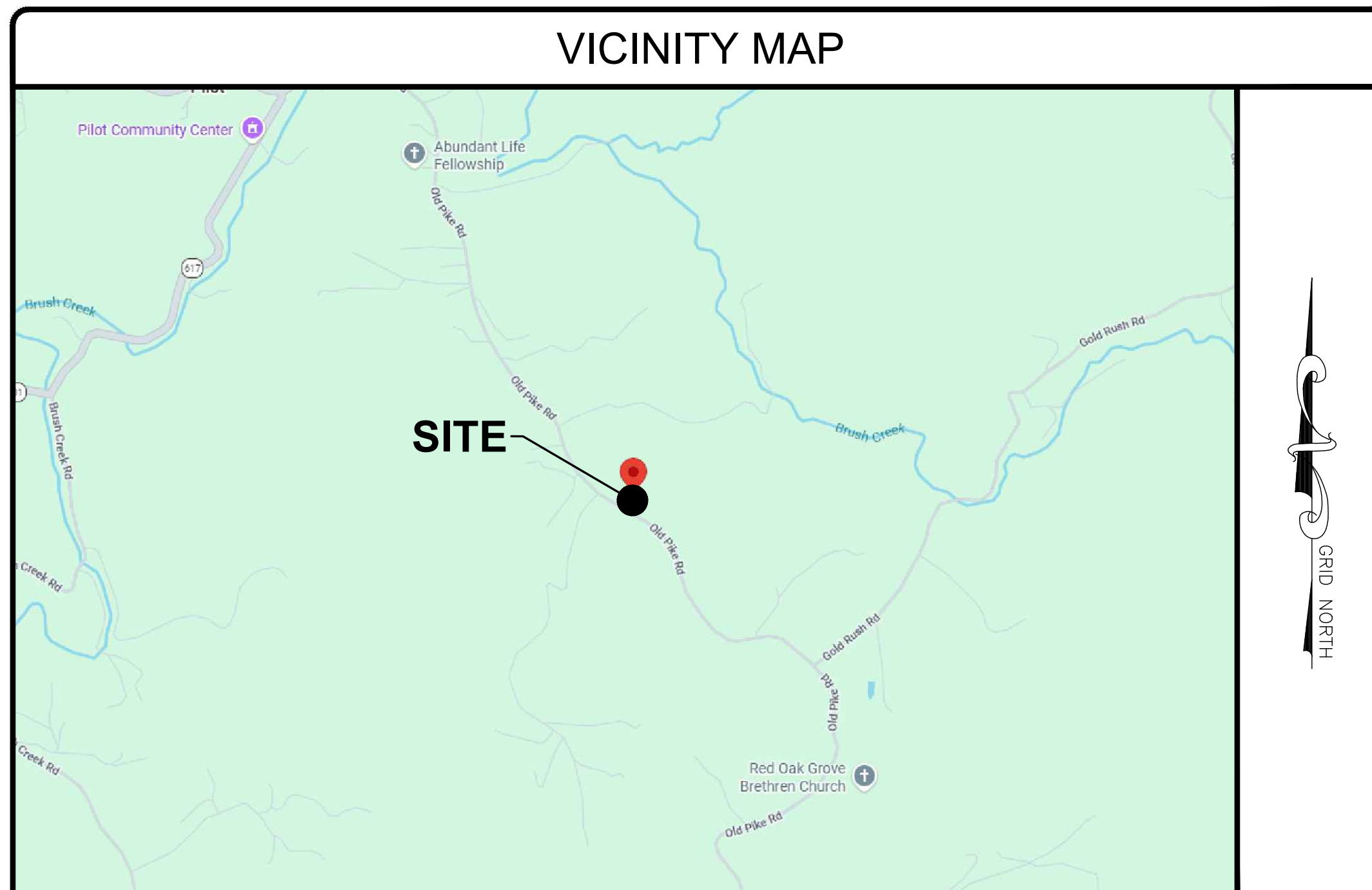
PROJECT DESCRIPTION:	RAWLAND: PROPOSED INSTALLATION OF TELECOMMUNICATIONS TOWER AND RELATED EQUIPMENT WITHIN SECURED COMPOUND.
SITE ADDRESS:	4752 OLD PIKE ROAD PILOT, VA 24138
LATITUDE (NAD 83):	37° 02' 27.2829" N (37.0409119°)
LONGITUDE (NAD 83):	80° 20' 56.5507" W (-80.3490418°)
GROUND ELEVATION:	2442.0' ± AMSL
JURISDICTION:	MONTGOMERY COUNTY
ZONING:	A1
PARCEL ID:	021247
PARCEL AREA:	124.24 ± ACRES
PARCEL OWNER:	WONDERLEY, JEAN M 4750 OLD PIKE RD PILOT, VA 24138
TOWER OWNER:	THE TOWERS, LLC 22 W ATLANTIC AVE, SUITE 310 DELRAY BEACH, FL 33444
STRUCTURE TYPE:	SELF-SUPPORT TOWER
HEIGHT OF STRUCTURE:	195.0' ± AGL
OVERALL HEIGHT OF STRUCTURE:	199.0' ± AGL
TOTAL LEASE AREA:	10,000 ± SQ. FT.
POWER PROVIDER:	APPALACHIAN POWER (800) 956-4237
TELCO PROVIDER:	VERIZON (800) 837-4966

### EMERGENCY INFORMATION:

FIRE & RESCUE DEPARTMENT:	(540) 382-6865
POLICE OFFICE:	(540) 382-3131

### PROJECT TEAM

APPLICANT:	VERTICAL BRIDGE 22 WEST ATLANTIC AVENUE, SUITE 310 DELRAY BEACH, FL 33444
PROJECT MANAGEMENT FIRM:	SITE LINK WIRELESS DON BART (856) 220-3453
CONSTRUCTION MANAGER:	VERTICAL BRIDGE BRIAN BELSKI O: (561) 223-8934 M: (803) 237-2451
ENGINEERING FIRM:	NB+C ENGINEERING SERVICES, LLC. 120 EASTSHORE DRIVE, SUITE 300 GLEN ALLEN, VA 23059 (804) 548-4079



### DIRECTIONS

FROM 2501 GOODES BRIDGE RD, RICHMOND, VA 23222:  
GET ON VA-150 N/CHIPPENHAM PKWY FROM GOODES BRIDGE RD AND US-360 E/HULL ST/HULL STREET RD, EXIT THE PARKING LOT, TURN RIGHT TOWARD GOODES BRIDGE RD, TURN RIGHT ONTO GOODES BRIDGE RD, TURN RIGHT ONTO US-360 E/HULL ST/HULL STREET RD, USE THE RIGHT LANE TO MERGE WITH VA-150 N/CHIPPENHAM PKWY VIA THE RAMP TO I-95 N/POWHITE PKWY, FOLLOW I-64 AND I-81 S TO VA-8 S/W MAIN ST IN CHRISTIANSBURG. TAKE EXIT 114 FROM I-81 S, JOIN WITH VA-150 N/CHIPPENHAM PKWY, USE THE RIGHT 2 LANES TO MERGE WITH VA-76 N/POWHITE PKWY, JOIN WITH I-195 N, KEEP LEFT TO STAY ON I-195 N, USE THE LEFT 2 LANES TO TAKE THE I-64 W EXIT TOWARD CHARLOTTESVILLE, JOIN WITH I-64, USE THE LEFT 2 LANES TO MERGE WITH I-64 W/I-81 S TOWARD I-64 W/LEXINGTON/ROANOKE, CONTINUE TO FOLLOW I-81 S TAKE EXIT 114 TOWARD FLOYD, TAKE STATE RTE 675 AND STATE RTE 615 TO YOUR DESTINATION IN PILOT, TURN LEFT ONTO VA-8 S/W MAIN ST, CONTINUE TO FOLLOW VA-8 S, TURN LEFT ONTO STATE RTE 675, TURN RIGHT ONTO STATE RTE 615, TURN RIGHT TO STAY ON STATE RTE 615, TURN LEFT AND ARRIVE AT DESTINATION.

### CODE COMPLIANCE

- ALL WORK AND MATERIALS SHALL BE PERFORMED AND INSTALLED IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE FOLLOWING CODES AS ADOPTED BY THE LOCAL GOVERNING AUTHORITIES. NOTHING IN THESE PLANS IS TO BE CONSTRUED TO PERMIT WORK NOT CONFORMING TO THE LATEST EDITIONS OF THE FOLLOWING CODES.
- 2021 VIRGINIA CONSTRUCTION CODE
  - 2020 NATIONAL ELECTRICAL CODE
  - 2021 NFPA 101, LIFE SAFETY CODE
  - 2021 VIRGINIA STATEWIDE FIRE PREVENTION CODE
  - AMERICAN CONCRETE INSTITUTE
  - AISC MANUAL OF STEEL CONSTRUCTION 15TH EDITION
  - ANSI/TIA-222-H
  - TIA 607
  - INSTITUTE FOR ELECTRICAL & ELECTRONICS ENGINEER 81
  - IEEE C2 NATIONAL ELECTRIC SAFETY CODE LATEST EDITION
  - TELCORDIA GR-1275
  - ANSI/TIA 311

### APPROVAL BLOCK

		APPROVED	APPROVED AS NOTED	DISAPPROVED /REVISE
VERTICAL BRIDGE	DATE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
SITE ACQUISITION	DATE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
CONSTRUCTION MANAGER	DATE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
ZONING	DATE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
RF ENGINEERING	DATE	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

### DRAWING INDEX

T-1	TITLE SHEET
T-2	GENERAL NOTES
EE-1	EASEMENT EXHIBIT
Z-1	SITE PLAN
Z-2	ADJACENT PROPERTY INFO
Z-3	ENLARGED SITE PLAN
C-1	COMPOUND PLAN
C-2	TOWER ELEVATION & NOTES
S-1	CONSTRUCTION DETAILS
S-2	FENCING DETAILS I
S-3	FENCING DETAILS II
S-4	SIGNAGE DETAILS

### DO NOT SCALE DRAWINGS

THESE DRAWINGS ARE FORMATTED TO BE SCALED AT FULL SIZE (22"X34") AND MAY NOT SCALE WHEN PRINTED AT OTHER SIZES. CONTRACTOR SHALL VERIFY ALL PLANS AND EXISTING DIMENSIONS AND CONDITIONS ON THE JOB SITE AND SHALL IMMEDIATELY NOTIFY THE DESIGNER / ENGINEER IN WRITING OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK OR MATERIAL ORDERS OR BE RESPONSIBLE FOR THE SAME. CONTRACTOR SHALL USE BEST MANAGEMENT PRACTICE TO PREVENT STORM WATER POLLUTION DURING CONSTRUCTION.

ENGINEERING FIRM



APPLICANT



CARRIER



SITE INFORMATION

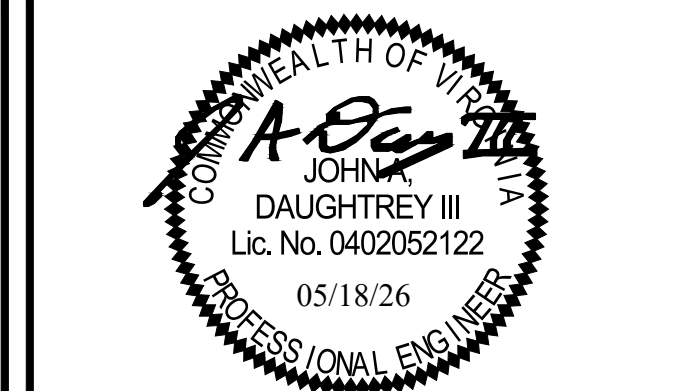
OLD PIKE  
VERTICAL BRIDGE RAWLAND  
SITE #: US-VA-5456  
NB+C PROJECT #: 100912  
4752 OLD PIKE ROAD  
PILOT, VA 24138  
MONTGOMERY COUNTY

DESIGN RECORD

### REVISIONS

REV	DATE	DESCRIPTION	BY
2	05/18/26	REVISED FINAL ZDS	JC
1	05/05/26	REVISED FINAL ZDS	JC
0	04/09/26	FINAL ZDS	SK
A	04/09/26	PRELIMINARY ZDS	SK

PROFESSIONAL STAMP



ENGINEER

JOHN A. DAUGHTREY III, P.E.  
VIRGINIA PROFESSIONAL ENGINEER  
LICENSE #0402052122

SHEET TITLE

TITLE SHEET

SHEET NUMBER

T-1

SITE ACTIVITY REQUIREMENTS:

- PRIOR TO THE START OF CONSTRUCTION, ALL REQUIRED JURISDICTIONAL PERMITS SHALL BE OBTAINED. THIS INCLUDES, BUT IS NOT LIMITED TO, BUILDING, ELECTRICAL, MECHANICAL, FIRE, FLOOD ZONE, ENVIRONMENTAL AND ZONING. AFTER ONSITE ACTIVITIES AND CONSTRUCTION ARE COMPLETED, ALL REQUIRED PERMITS SHALL BE SATISFIED AS CLOSED OUT ACCORDING TO LOCAL JURISDICTIONAL REQUIREMENTS.
- ALL CONSTRUCTION MEANS AND METHODS, INCLUDING BUT NOT LIMITED TO, ERECTION PLANS, RIGGING PLANS, CLIMBING PLANS, AND RESCUE PLANS SHALL BE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR RESPONSIBLE FOR THE EXECUTION OF THE WORK CONTAINED HEREON, AND SHALL MEET ANIS/ASSE A10.48 (LATEST EDITION); FEDERAL, STATE, AND LOCAL REGULATIONS; AND ANY APPLICABLE INDUSTRY CONSENSUS STANDARDS RELATED TO THE CONSTRUCTION ACTIVITIES BEING PERFORMED. ALL RIGGING PLANS SHALL ADHERE TO ANIS/ASSE A10.48 (LATEST EDITION) AND THE REQUIRED INVOLVEMENT OF A QUALIFIED ENGINEER FOR CLASS IV CONSTRUCTION, TO CERTIFY THE SUPPORTING STRUCTURE(S) IN ACCORDANCE WITH ANSI/TIA-322 (LATEST EDITION).
- ALL SITE WORK TO COMPLY WITH OAS-STD-10068 "INSTALLATION STANDARDS FOR CONSTRUCTION ACTIVITIES" STANDARD FOR INSTALLATION OF MOUNTS AND APPURTENANCES, AND LATEST VERSION OF ANSI/TIA-1019-A-2012 "STANDARD FOR INSTALLATION, ALTERATION, AND MAINTENANCE OF ANTENNA SUPPORTING STRUCTURES AND ANTENNAS."
- IF THE SPECIFIED EQUIPMENT CAN NOT BE INSTALLED AS SHOWN ON THESE DRAWINGS, THE CONTRACTOR SHALL PROPOSE AN ALTERNATIVE INSTALLATION FOR APPROVAL BY TOWER OWNER PRIOR TO PROCEEDING WITH ANY SUCH CHANGE OF INSTALLATION.
- ALL MATERIALS FURNISHED AND INSTALLED SHALL BE IN STRICT ACCORDANCE WITH ALL APPLICABLE CODES REGULATIONS AND ORDINANCES. CONTRACTOR SHALL ISSUE ALL APPROPRIATE NOTICES AND COMPLY WITH ALL LAWS, ORDINANCES, RULES, REGULATIONS AND LAWFUL ORDERS OF ANY PUBLIC AUTHORITY REGARDING THE PERFORMANCE OF THE WORK. ALL WORK CARRIED OUT SHALL COMPLY WITH ALL APPLICABLE MUNICIPAL AND UTILITY COMPANY SPECIFICATIONS AND LOCAL JURISDICTIONAL CODES, ORDINANCES AND APPLICABLE REGULATIONS.
- THE CONTRACTOR SHALL INSTALL ALL EQUIPMENT AND MATERIALS IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS UNLESS SPECIFICALLY STATED OTHERWISE.
- THE CONTRACTOR SHALL CONTACT UTILITY LOCATING SERVICES PRIOR TO THE START OF CONSTRUCTION.
- ALL EXISTING ACTIVE SEWER, WATER, GAS, ELECTRIC AND OTHER UTILITIES WHERE ENCOUNTERED IN THE WORK, SHALL BE PROTECTED AT ALL TIMES AND WHERE REQUIRED FOR THE PROPER EXECUTION OF THE WORK, SHALL BE RELOCATED AS DIRECTED BY CONTRACTOR. EXTREME CAUTION SHOULD BE USED BY THE CONTRACTOR WHEN EXCAVATING OR DRILLING PIERS AROUND OR NEAR UTILITIES. CONTRACTOR SHALL PROVIDE SAFETY TRAINING FOR THE WORKING CREW. THIS WILL INCLUDE BUT NOT BE LIMITED TO A) FALL PROTECTION B) CONFINED SPACE C) ELECTRICAL SAFETY D) TRENCHING AND EXCAVATION E) CONSTRUCTION SAFETY PROCEDURES.
- ALL SITE WORK SHALL BE AS INDICATED ON THE STAMPED CONSTRUCTION DRAWINGS AND PROJECT SPECIFICATIONS, LATEST APPROVED REVISION.
- CONTRACTOR SHALL KEEP THE SITE FREE FROM ACCUMULATING WASTE MATERIAL, DEBRIS, AND TRASH AT THE COMPLETION OF THE WORK. IF NECESSARY, RUBBISH, STUMPS, DEBRIS, STICKS, STONES AND OTHER REFUSE SHALL BE REMOVED FROM THE SITE AND DISPOSED OF LEGALLY.
- ALL EXISTING INACTIVE SEWER, WATER, GAS, ELECTRIC AND OTHER UTILITIES, WHICH INTERFERE WITH THE EXECUTION OF THE WORK, SHALL BE REMOVED AND/OR CAPPED, PLUGGED OR OTHERWISE DISCONTINUED AT POINTS WHICH WILL NOT INTERFERE WITH THE EXECUTION OF THE WORK, SUBJECT TO THE APPROVAL OF CONTRACTOR, TOWER OWNER AND/OR LOCAL UTILITIES.
- THE CONTRACTOR SHALL PROVIDE SITE SIGNAGE IN ACCORDANCE WITH THE TECHNICAL SPECIFICATION FOR SITE SIGNAGE REQUIRED BY LOCAL JURISDICTION AND SIGNAGE REQUIRED ON INDIVIDUAL PIECES OF EQUIPMENT, ROOMS, AND SHELTERS.
- THE SITE SHALL BE GRADED TO CAUSE SURFACE WATER TO FLOW AWAY FROM THE CARRIER'S EQUIPMENT AND TOWER AREAS.
- THE SUB GRADE SHALL BE COMPACTED AND BROUGHT TO A SMOOTH UNIFORM GRADE PRIOR TO FINISHED SURFACE APPLICATION.
- THE AREAS OF THE OWNERS PROPERTY DISTURBED BY THE WORK AND NOT COVERED BY THE TOWER, EQUIPMENT OR DRIVEWAY, SHALL BE GRADED TO A UNIFORM SLOPE, AND STABILIZED TO PREVENT EROSION AS SPECIFIED ON THE CONSTRUCTION DRAWINGS AND/OR PROJECT SPECIFICATIONS.
- CONTRACTOR SHALL MINIMIZE DISTURBANCE TO EXISTING SITE DURING CONSTRUCTION. EROSION CONTROL MEASURES, IF REQUIRED DURING CONSTRUCTION, SHALL BE IN CONFORMANCE WITH THE LOCAL GUIDELINES FOR EROSION AND SEDIMENT CONTROL.
- THE CONTRACTOR SHALL PROTECT EXISTING IMPROVEMENTS, PAVEMENTS, CURBS, LANDSCAPING AND STRUCTURES. ANY DAMAGED PART SHALL BE REPAIRED AT CONTRACTOR'S EXPENSE TO THE SATISFACTION OF OWNER.
- CONTRACTOR SHALL LEGALLY AND PROPERLY DISPOSE OF ALL SCRAP MATERIALS SUCH AS COAXIAL CABLES AND OTHER ITEMS REMOVED FROM THE EXISTING FACILITY. ANTENNAS REMOVED SHALL BE RETURNED TO THE OWNER'S DESIGNATED LOCATION.
- CONTRACTOR SHALL LEAVE PREMISES IN CLEAN CONDITION. TRASH AND DEBRIS SHOULD BE REMOVED FROM SITE ON A DAILY BASIS.
- NO FILL OR EMBANKMENT MATERIAL SHALL BE PLACED ON FROZEN GROUND. FROZEN MATERIALS, SNOW OR ICE SHALL NOT BE PLACED IN ANY FILL OR EMBANKMENT.

GROUNDING NOTES:




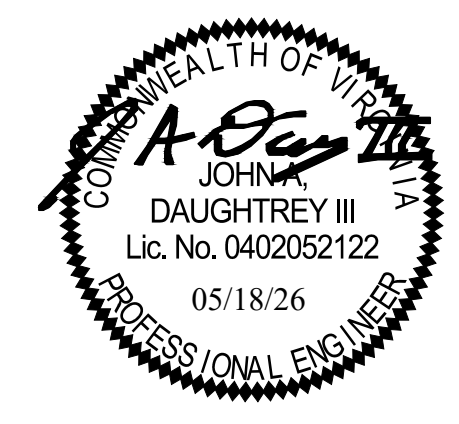
- ALL GROUND ELECTRODE SYSTEMS (INCLUDING TELECOMMUNICATIONS, RADIO, LIGHTNING PROTECTION AND AC POWER GES'S) SHALL BE BONDED TOGETHER AT OR BELOW GRADE, BY TWO OR MORE COPPER BONDING CONDUCTORS IN ACCORDANCE WITH THE NEC.
- THE CONTRACTOR SHALL PERFORM IEEE FALL-OF-POTENTIAL RESISTANCE TO EARTH TESTING (PER IEEE 1100 AND 81 STANDARDS) FOR GROUND ELECTRODE SYSTEMS, THE CONTRACTOR SHALL FURNISH AND INSTALL SUPPLEMENTAL GROUND ELECTRODES AS NEEDED TO ACHIEVE A TEST RESULT OF 5 OHMS OR LESS.
- THE CONTRACTOR IS RESPONSIBLE FOR PROPERLY SEQUENCING GROUNDING AND UNDERGROUND CONDUIT INSTALLATION AS TO PREVENT ANY LOSS OF CONTINUITY IN THE GROUNDING SYSTEM OR DAMAGE TO THE CONDUIT AND PROVIDE TESTING RESULTS.
- METAL CONDUIT AND TRAY SHALL BE GROUND AND MADE ELECTRICALLY CONTINUOUS WITH LISTED BONDING FITTINGS OR BY BONDING ACROSS THE DISCONTINUITY WITH #6 AWG COPPER WIRE UL APPROVED GROUNDING TYPE CONDUIT CLAMPS.
- METAL RACEWAY SHALL NOT BE USED AS THE NEC REQUIRED EQUIPMENT GROUND CONDUCTOR. STRANDED COPPER CONDUCTORS WITH GREEN INSULATION, SIZED IN ACCORDANCE WITH THE NEC, SHALL BE FURNISHED AND INSTALLED WITH THE POWER CIRCUITS TO BTS EQUIPMENT.
- EACH CABINET FRAME SHALL BE DIRECTLY CONNECTED TO THE MASTER GROUND BAR WITH GREEN INSULATED SUPPLEMENTAL EQUIPMENT GROUND WIRES, #6 AWG STRANDED COPPER OR LARGER FOR INDOOR BTS; #2 AWG BARE SOLID TINNED COPPER FOR OUTDOOR BTS.
- CONNECTIONS TO THE GROUND BUS SHALL NOT BE DOUBLED UP OR STACKED BACK TO BACK. CONNECTIONS ON OPPOSITE SIDE OF THE GROUND BUS ARE PERMITTED.
- ALL EXTERIOR GROUND CONDUCTORS BETWEEN EQUIPMENT/GROUND BARS AND THE GROUND RING SHALL BE #2 AWG SOLID TINNED COPPER UNLESS OTHERWISE INDICATED.
- ALUMINUM CONDUCTOR OR COPPER CLAD STEEL CONDUCTOR SHALL NOT BE USED FOR GROUNDING CONNECTIONS.
- USE OF 90° BENDS IN THE PROTECTION GROUNDING CONDUCTORS SHALL BE AVOIDED WHEN 45° BENDS CAN BE ADEQUATELY SUPPORTED.
- EXOTHERMIC WELDS SHALL BE USED FOR ALL GROUNDING CONNECTIONS BELOW GRADE.
- ALL GROUND CONNECTIONS ABOVE GRADE (INTERIOR AND EXTERIOR) SHALL BE FORMED USING HIGH PRESS CRIMPS.
- COMPRESSION GROUND CONNECTIONS MAY BE REPLACED BY EXOTHERMIC WELD CONNECTIONS.
- ICE BRIDGE BONDING CONDUCTORS SHALL BE EXOTHERMICALLY BONDED OR BOLTED TO THE BRIDGE AND THE TOWER GROUND BAR.
- APPROVED ANTIOXIDANT COATINGS (I.E. CONDUCTIVE GEL OR PASTE) SHALL BE USED ON ALL COMPRESSION AND BOLTED GROUND CONNECTIONS.
- ALL EXTERIOR GROUND CONNECTIONS SHALL BE COATED WITH A CORROSION RESISTANT MATERIAL.
- MISCELLANEOUS ELECTRICAL AND NON-ELECTRICAL METAL BOXES, FRAMES AND SUPPORTS SHALL BE BONDED TO THE GROUND RING, IN ACCORDANCE WITH THE NEC.
- BOND ALL METALLIC OBJECTS WITHIN 6FT OF MAIN GROUND RING WITH (1) #2 AWG BARE SOLID TINNED COPPER GROUND CONDUCTOR.
- GROUND CONDUCTORS USED FOR THE FACILITY GROUNDING AND LIGHTNING PROTECTION SYSTEMS SHALL NOT BE ROUTED THROUGH METALLIC OBJECTS THAT FORM A RING AROUND THE CONDUCTOR, SUCH AS METALLIC CONDUITS, METAL SUPPORT CUPS OR SLEEVES THROUGH WALLS OR FLOORS. WHEN IT IS REQUIRED TO BE HOUSED IN CONDUIT TO MEET CODE REQUIREMENTS OR LOCAL CONDITIONS, NON-METALLIC MATERIAL SUCH AS PVC CONDUIT SHALL BE USED. WHERE USE OF METAL CONDUIT IS UNAVOIDABLE (I.E., NONMETALLIC CONDUIT PROHIBITED BY LOCAL CODE) THE GROUND CONDUCTOR SHALL BE BONDED TO EACH END OF THE METAL CONDUIT.
- ALL GROUNDS THAT TRANSITION FROM BELOW GRADE TO ABOVE GRADE MUST BE #2 AWG BARE SOLID TINNED COPPER N 3/4" NON-METALLIC, FLEXIBLE CONDUIT FROM 24" BELOW GRADE TO WITHIN 3" TO 6" OF CAD-WELD TERMINATION POINT. THE EXPOSED END OF THE CONDUIT MUST BE SEALED WITH SILICONE CAULK. (ADD TRANSITIONING GROUND STANDARD DETAIL AS WELL).
- BUILDINGS WHERE THE MAIN GROUNDING CONDUCTORS ARE REQUIRED TO BE ROUTED TO GRADE, THE CONTRACTOR SHALL ROUTE TWO GROUNDING CONDUCTORS FROM THE ROOFTOP, TOWERS, AND WATER TOWERS GROUNDING RING, TO THE EXISTING GROUNDING SYSTEM, THE GROUNDING CONDUCTORS SHALL NOT BE SMALLER THAN #2/0 AWG COPPER. ROOFTOP GROUNDING RING SHALL BE BONDED TO THE EXISTING GROUNDING SYSTEM, THE BUILDING STEEL COLUMNS, LIGHTNING PROTECTION SYSTEM, AND BUILDING MAIN WATER LINE (FERROUS OR NONFERROUS METAL PIPING ONLY).

CONCRETE, FOUNDATIONS, AND REINFORCING STEEL:

- ALL CONCRETE WORK SHALL BE IN ACCORDANCE WITH THE AC 301, AC 318, ACI 336, ASTM A184, ASTM A185 AND THE DESIGN AND CONSTRUCTION SPECIFICATION FOR CAST-IN-PLACE CONCRETE.
- UNLESS NOTED OTHERWISE, SOIL BEARING PRESSURE USED FOR DESIGN OF SLABS AND FOUNDATIONS IS ASSUMED TO BE 1000 PSF.
- ALL CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3000 PSI AT 28 DAYS, UNLESS NOTED OTHERWISE. NO MORE THAN 90 MINUTES SHALL PASS FROM BATCH TIME TO TIME OF PLACEMENT UNLESS APPROVED BY THE ENGINEER OF RECORD. TEMPERATURE OF CONCRETE SHALL NOT EXCEED 90 DEGREE FAHRENHEIT AT TIME OF PLACEMENT.
- CONCRETE EXPOSED TO FREEZE-THAW CYCLES SHALL CONTAIN AIR ENTRAINING ADMIXTURES. AMOUNT OF AIR ENTRAINMENT TO BE BASED ON SIZE OF AGGREGATE AND F3 CLASS EXPOSURE (VERY SEVERE). CEMENT USED TO BE TYPE II PORTLAND CEMENT WITH A MAXIMUM WATER-TO-CEMENT RATIO (W/C) OF 0.45.
- ALL STEEL REINFORCING SHALL CONFORM TO ASTM A615, ALL WELDED WIRE FABRIC (WWF) SHALL CONFORM TO ASTM A185. ALL SPLICES SHALL BE CLASS "B" TENSION SPLICES, UNLESS NOTED OTHERWISE. ALL HOOKS SHALL BE STANDARD 90° HOOKS, UNLESS NOTED OTHERWISE. YIELD STRENGTH (FY) OF STANDARD DEFORMED BARS ARE AS FOLLOW:  
#4 BARS AND SMALLER.....40 KSI  
#5 BARS AND LARGER.....60 KSI
- THE FOLLOWING MINIMUM CONCRETE COVER SHALL BE PROVIDED FOR REINFORCING STEEL UNLESS SHOWN OTHERWISE ON DRAWINGS:  
CONCRETE CAST AGAINST AND PERMANENTLY EXPOSED TO EARTH..... 3"  
CONCRETE EXPOSED TO EARTH OR WEATHER:  
#6 BARS AND LARGER.....2"  
#5 BARS AND SMALLER.....1-1/2"  
7. CONCRETE NOT EXPOSED TO EARTH OR WEATHER:  
SLAB AND WALLS.....3/4"  
BEAMS AND COLUMNS.....1-1/2"
- A TOOLED EDGE OR A 3/4 CHAMFER SHALL BE PROVIDED AT ALL EXPOSED EDGES OF CONCRETE, UNLESS NOTED OTHERWISE, IN ACCORDANCE WITH ACI 301 SECTION 4.2.4.

GENERAL NOTES:

- THESE DRAWINGS HAVE BEEN PREPARED USING STANDARDS OF PROFESSIONAL CARE AND COMPLETENESS NORMALLY EXERCISED UNDER SIMILAR CIRCUMSTANCES BY REPUTABLE ENGINEERS IN THIS OR SIMILAR LOCALITIES. ITS ASSUMED THAT THE WORK DEPICTED WILL BE PERFORMED BY AN EXPERIENCED CONTRACTOR AND/OR WORKPEOPLE WHO HAVE A WORKING KNOWLEDGE OF THE APPLICABLE CODE STANDARDS AND REQUIREMENTS AND OF INDUSTRY ACCEPTED STANDARD GOOD PRACTICE. AS NOT EVERY CONDITION OR ELEMENT IS (OR CAN BE) EXPLICITLY SHOWN ON THESE DRAWINGS, THE CONTRACTOR SHALL USE INDUSTRY ACCEPTED STANDARD GOOD PRACTICE FOR MISCELLANEOUS WORK NOT EXPLICITLY SHOWN.
- THESE DRAWINGS REPRESENT THE FINISHED STRUCTURE. THEY DO NOT INDICATE THE MEANS OR METHODS OF CONSTRUCTION. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES, AND PROCEDURES. THE CONTRACTOR SHALL PROVIDE ALL MEASURES NECESSARY FOR PROTECTION OF LIFE AND PROPERTY DURING CONSTRUCTION. SUCH MEASURES SHALL INCLUDE, BUT NOT LIMITED TO, BRACING, FRAMEWORK, SHORING, ETC. SITE VISITS BY THE ENGINEER OR HIS REPRESENTATIVE WILL NOT INCLUDE INSPECTION OF THESE ITEMS AND IS FOR STRUCTURAL OBSERVATION OF THE FINISHED STRUCTURE ONLY.
- NOTES AND DETAILS IN THE CONSTRUCTION DRAWINGS SHALL TAKE PRECEDENCE OVER GENERAL NOTES AND TYPICAL DETAILS. WHERE NO DETAILS ARE SHOWN, CONSTRUCTION SHALL CONFORM TO SIMILAR WORK ON THE PROJECT, AND/OR AS PROVIDED FOR IN THE CONTRACT DOCUMENTS. WHERE DISCREPANCIES OCCUR BETWEEN PLANS, DETAILS, GENERAL NOTES, AND SPECIFICATIONS, THE GREATER, MORE STRICT REQUIREMENTS, SHALL GOVERN. IF FURTHER CLARIFICATION IS REQUIRED CONTACT THE ENGINEER OF RECORD.
- SUBSTANTIAL EFFORT HAS BEEN MADE TO PROVIDE ACCURATE DIMENSIONS AND MEASUREMENTS ON THE DRAWINGS TO ASSIST IN THE FABRICATION AND/OR PLACEMENT OF CONSTRUCTION ELEMENTS BUT IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY THE DIMENSIONS, MEASUREMENTS, AND/OR CLEARANCES SHOWN IN THE CONSTRUCTION DRAWINGS PRIOR TO FABRICATION OR CUTTING OF ANY NEW OR EXISTING CONSTRUCTION ELEMENTS.
- IF IT IS DETERMINED THAT THERE ARE DISCREPANCIES AND/OR CONFLICTS WITH THE CONSTRUCTION DRAWINGS THE ENGINEER OF RECORD IS TO BE NOTIFIED AS SOON AS POSSIBLE
- PRIOR TO THE SUBMISSION OF BIDS, THE BIDDING CONTRACTOR SHALL VISIT THE CELL SITE TO FAMILIARIZE WITH THE EXISTING CONDITIONS AND TO CONFIRM THAT THE WORK CAN BE ACCOMPLISHED AS SHOWN ON THE CONSTRUCTION DRAWINGS. ANY DISCREPANCY FOUND SHALL BE BROUGHT TO THE ATTENTION OF THE TOWER OWNER.
- ALL MATERIALS FURNISHED AND INSTALLED SHALL BE IN STRICT ACCORDANCE WITH ALL APPLICABLE CODES, REGULATIONS AND ORDINANCES. CONTRACTOR SHALL ISSUE ALL APPROPRIATE NOTICES AND COMPLY WITH ALL LAWS, ORDINANCES, RULES, REGULATIONS AND LAWFUL ORDERS OF ANY PUBLIC AUTHORITY REGARDING THE PERFORMANCE OF THE WORK. ALL WORK CARRIED OUT SHALL COMPLY WITH ALL APPLICABLE MUNICIPAL AND UTILITY COMPANY SPECIFICATIONS AND LOCAL JURISDICTIONAL CODES, ORDINANCES AND APPLICABLE REGULATIONS.
- UNLESS NOTED OTHERWISE, THE WORK SHALL INCLUDE FURNISHING MATERIALS, EQUIPMENT, APPURTENANCES AND LABOR NECESSARY TO COMPLETE ALL INSTALLATIONS AS INDICATED ON THE DRAWINGS. THE CONTRACTOR SHALL INSTALL ALL EQUIPMENT AND MATERIALS IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS UNLESS SPECIFICALLY STATED OTHERWISE.
- IF THE SPECIFIED EQUIPMENT CAN NOT BE INSTALLED AS SHOWN ON THESE DRAWINGS, THE CONTRACTOR SHALL PROPOSE AN ALTERNATIVE INSTALLATION FOR APPROVAL BY THE CARRIER AND TOWER OWNER PRIOR TO PROCEEDING WITH ANY SUCH CHANGE OF INSTALLATION.
- CONTRACTOR IS TO PERFORM A SITE INVESTIGATION AND IS TO DETERMINE THE BEST ROUTING OF ALL CONDUITS FOR POWER, TELCO AND FOR GROUNDING CABLES AS SHOWN IN THE POWER, TELCO, AND GROUNDING PLAN.
- THE CONTRACTOR SHALL PROTECT EXISTING IMPROVEMENTS, PAVEMENTS, CURBS, LANDSCAPING AND STRUCTURES. ANY DAMAGED PART SHALL BE REPAIRED AT CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE TOWER OWNER.
- CONTRACTOR SHALL LEGALLY AND PROPERLY DISPOSE OF ALL SCRAP MATERIALS SUCH AS COAXIAL CABLES AND OTHER ITEMS REMOVED FROM THE EXISTING FACILITY. ANTENNAS REMOVED SHALL BE RETURNED TO THE OWNER'S DESIGNATED LOCATION.
- CONTRACTOR SHALL LEAVE PREMISES IN CLEAN CONDITION. TRASH AND DEBRIS SHOULD BE REMOVED FROM SITE ON A DAILY BASIS.

ENGINEERING FIRM	 <p><b>TOTALLY COMMITTED.</b></p> <p><b>NB+C ENGINEERING SERVICES, LLC.</b> 120 EASTSHORE DRIVE, SUITE 300 GLEN ALLEN, VA 23059 (804) 548-4079</p>																						
APPLICANT	 <p><b>verticalbridge</b> THE TOWERS, LLC</p>																						
CARRIER																							
SITE INFORMATION	<p>OLD PIKE VERTICAL BRIDGE RAWLAND SITE #: US-VA-5456 NB+C PROJECT #: 100912 4752 OLD PIKE ROAD PILOT, VA 24138 MONTGOMERY COUNTY</p>																						
DESIGN RECORD	<p>REVISIONS</p> <table border="1"> <thead> <tr> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> </tr> </thead> <tbody> <tr> <td>2</td> <td>05/18/26</td> <td>REVISED FINAL ZDS</td> <td>JC</td> </tr> <tr> <td>1</td> <td>05/05/26</td> <td>REVISED FINAL ZDS</td> <td>JC</td> </tr> <tr> <td>0</td> <td>04/09/26</td> <td>FINAL ZDS</td> <td>SK</td> </tr> <tr> <td>A</td> <td>04/09/26</td> <td>PRELIMINARY ZDS</td> <td>SK</td> </tr> </tbody> </table>			REV	DATE	DESCRIPTION	BY	2	05/18/26	REVISED FINAL ZDS	JC	1	05/05/26	REVISED FINAL ZDS	JC	0	04/09/26	FINAL ZDS	SK	A	04/09/26	PRELIMINARY ZDS	SK
REV	DATE	DESCRIPTION	BY																				
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0	04/09/26	FINAL ZDS	SK																				
A	04/09/26	PRELIMINARY ZDS	SK																				
PROFESSIONAL STAMP																							
ENGINEER	<p>JOHN A. DAUGHTREY III, P.E. VIRGINIA PROFESSIONAL ENGINEER LICENSE #0402052122</p>																						
SHEET TITLE	<p><b>GENERAL NOTES</b></p>																						
SHEET NUMBER	<p><b>T-2</b></p>																						

THIS EXHIBIT DOES NOT CONSTITUTE A BOUNDARY SURVEY AND IS FOR THE EXPRESS PURPOSE OF SHOWING PROPOSED LEASE AREAS.

**LEGAL DESCRIPTION LESSEE 30' UTILITIES EASEMENT**

A LESSEE 30' UTILITIES EASEMENT OVER AND ACROSS THAT 120 ± ACRE PARCEL AS DESCRIBED IN INSTRUMENT 2011008161, OF RECORD IN THE CLERK OF THE CIRCUIT COURTS OFFICE, MONTGOMERY COUNTY, VIRGINIA; BEING PARCEL ID 021247, LYING ALONG THE NORTHERLY RIGHT OF WAY OF STATE ROUTE 615, LOCALLY KNOWN AS OLD PIKE ROAD, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT BEING THE SOUTHEASTERLY MOST CORNER OF THE LEASE AREA HAVING A COORDINATE VALUE OF N: 3,543,670.43 BY E: 10,943,298.07 AS RELATED TO THE VIRGINIA STATE PLANE COORDINATE SYSTEM (NAD 83-2011), SOUTH ZONE;

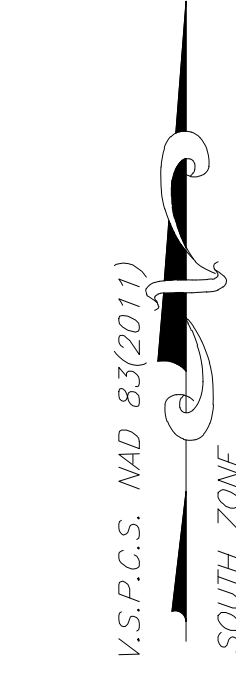
THENCE S 31°08'02" W A DISTANCE OF 10.00' TO A POINT;  
 THENCE N 58°51'58" W A DISTANCE OF 35.00' TO A POINT;  
 THENCE S 31°08'02" W A DISTANCE OF 17.55' TO A POINT;  
 THENCE S 68°05'18" W A DISTANCE OF 43.76' TO A POINT;  
 THENCE S 85°50'45" W A DISTANCE OF 39.47' TO A POINT;  
 THENCE S 32°20'45" W A DISTANCE OF 20.84' TO A POINT;  
 THENCE N 57°39'15" W A DISTANCE OF 30.00' TO A POINT;  
 THENCE N 32°20'45" E A DISTANCE OF 35.96' TO A POINT;  
 THENCE N 85°50'45" E A DISTANCE OF 49.90' TO A POINT;  
 THENCE N 68°05'18" E A DISTANCE OF 29.05' TO A POINT;  
 THENCE N 31°08'02" E A DISTANCE OF 17.53' TO A POINT;  
 THENCE S 58°51'58" E A DISTANCE OF 65.00' TO A POINT WHICH IS THE POINT OF BEGINNING, HAVING AN AREA OF 4,311 SQUARE FEET, 0.099 ACRES.

**LEGAL DESCRIPTION LESSEE'S 100' x 100' LEASE AREA**

A LESSEE 100' x 100' LEASE AREA ON THAT 120 ± ACRE PARCEL AS DESCRIBED IN INSTRUMENT 2011008161, OF RECORD IN THE CLERK OF THE CIRCUIT COURTS OFFICE, MONTGOMERY COUNTY, VIRGINIA; BEING PARCEL ID 021247, LYING ALONG THE NORTHERLY RIGHT OF WAY OF STATE ROUTE 615, LOCALLY KNOWN AS OLD PIKE ROAD, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT BEING THE SOUTHEASTERLY MOST CORNER OF THE LEASE AREA HAVING A COORDINATE VALUE OF N: 3,543,670.43 BY E: 10,943,298.07 AS RELATED TO THE VIRGINIA STATE PLANE COORDINATE SYSTEM (NAD 83-2011), SOUTH ZONE;

THENCE N 31°08'02" W A DISTANCE OF 100.00' TO A POINT;  
 THENCE N 58°51'58" E A DISTANCE OF 100.00' TO A POINT;  
 THENCE S 31°08'02" E A DISTANCE OF 100.00' TO A POINT;  
 THENCE S 58°51'58" W A DISTANCE OF 100.00' TO A POINT BEING THE TRUE POINT AND PLACE OF BEGINNING, HAVING AN AREA OF 10,000 SQUARE FEET OR 0.230 ACRES.



**CERTIFICATION**

I, ROBERT T. WILLIAMS, JR., A LICENSED VIRGINIA LAND SURVEYOR HEREBY CERTIFY TO THE FOLLOWING:

Vertical Bridge REIT, LLC, a Delaware limited liability company, its subsidiaries, and their respective successors and/or assigns, its lenders, and administrative agents and each of their respective successors and/or assigns, and Tower Title, LLC.

THIS SURVEYOR HAS RECEIVED AND REVIEWED THAT CERTAIN TITLE REPORT: BY TOWER TITLE & CLOSING DATED JANUARY 5, 2026, VERTICAL BRIDGE FILE #US-VA-5456 FOR PARCEL ID 021247, WHICH PROPOSES TO INSURE THE LANDS DESCRIBED IN SAID TITLE REPORT. THE SAID TITLE REPORT DOES DESCRIBE THE LANDS AS DEPICTED ON THIS EXHIBIT.

SCHEDULE B--PART II EASEMENTS AND/OR RIGHTS OF WAY:  
 11. ANY ALL MATTERS DISCLOSED ON THE MAP ENTITLED "ELLEN A. GUERRANT-MAP AND CERTIFICATE" DATED SEPTEMBER 27, 1945 AND RECORDED SEPTEMBER 27, 1945 IN BOOK 137 PAGE 324 IN MONTGOMERY COUNTY, VIRGINIA. (NOTHING SHOWN ON PLAT THE AFFECTS THE LEASE OR ACCESS)

**EXHIBIT NOTES**

- TOPOGRAPHIC SURVEY PERFORMED BY TIM FALLON LAND SURVEYING, PLLC 15139 CARROLLTON, VIRGINIA, 23314. 757-837-2919. SURVEY DATE: MARCH 5, 2026.
- COORDINATES AND ELEVATIONS SHOWN WERE ESTABLISHED USING KINEMATIC GPS OBSERVATIONS, PROVIDED THROUGH THE TOPNET LIVE GEODETIC CONTROL NETWORK. VERTICAL DATUM - NAVD 88 HORIZONTAL REFERENCE FRAME - NAD 83(2011) DISTANCES SHOWN ARE ON THE U.S. SURVEY FOOT
- NO SUB-SURFACE INVESTIGATION WAS PERFORMED BY TIM FALLON LAND SURVEYING, PLLC THIS EXHIBIT DOES NOT GUARANTEE THE "EXISTENCE OR NONEXISTENCE" OF UNDERGROUND UTILITIES. PRIOR TO ANY CONSTRUCTION OR EXCAVATION, CONTACT MISS UTILITY AT 1-800-552-7001 TO CONFIRM THE LOCATION OR EXISTENCE OF UNDERGROUND UTILITIES.
- THIS EXHIBIT WAS DONE WITH THE BENEFIT OF A TITLE REPORT BY TOWER TITLE & CLOSING, DATED JANUARY 5, 2026, VERTICAL BRIDGE FILE #US-VA-5456 TOWER TITLE, LLC FILE #VTB-231662-C.
- PROPERTY IS SUBJECT TO ALL EASEMENTS AND RESTRICTIONS OF RECORD.
- THIS EXHIBIT DOES NOT REPRESENT A BOUNDARY SURVEY. THE RIGHT-OF-WAY, PROPERTY LINE AND/OR EASEMENTS SHOWN HEREON REPRESENT A COMPILATION OF RECORDED DEEDS, PLATS, G.I.S. RECORDS AND TAX MAPS.
- THE AREA OF THE PROPOSED CELL TOWER APPEARS TO LIE WITHIN FLOOD ZONE "X", ACCORDING TO THE FEDERAL EMERGENCY MANAGEMENT AGENCY - NATIONAL FLOOD INSURANCE COMMUNITY NUMBER 51121C-0265 C, DATED SEPTEMBER 25, 2009.
- THE EXISTENCE OF HAZARDOUS WASTE, VEGETATED WETLANDS, OR TIDAL WETLANDS, WAS NEITHER INVESTIGATED, NOR CONFIRMED DURING THE PERFORMANCE OF THIS EXHIBIT.
- ACCESS AND UTILITY EASEMENTS TERMINATE AT A CONFIRMED PUBLIC ROW.

PROPOSED 195' (AGL) MONOPOLE TOWER  
 LAT: 37°02'27.2829"  
 LONG: 80°20'56.5507"  
 ELEV. 2442

N/F  
 JOHN E. WONDERLY &  
 LISA E.W. FORSTER  
 INST. 2011008161  
 PARCEL ID 021247

**LINE TABLE**

LINE	BEARING	DISTANCE
L1	N 58°51'58" W	100.00'
L2	N 31°08'02" E	100.00'
L3	S 58°51'58" E	100.00'
L4	S 31°08'02" W	100.00'
L5	S 31°08'02" W	10.00'
L6	N 58°51'58" W	35.00'
L7	S 31°08'02" W	17.55'
L8	S 68°05'18" W	43.76'
L9	S 85°50'45" W	39.47'
L10	S 32°20'45" W	20.84'
L11	N 57°39'15" W	30.00'
L12	N 32°20'45" E	35.96'
L13	N 85°50'45" E	49.90'
L14	N 68°05'18" E	29.05'
L15	N 31°08'02" E	17.53'
L16	S 58°51'58" E	65.00'

AT THE TIME OF THE TOPOGRAPHIC SURVEY THERE WERE NO VISIBLE ENCROACHMENTS LOCATED ON THE LEASE AREA. THE LEASE AND EASEMENT AREAS LIE ENTIRELY WITHIN THE PARENT PARCEL.

**TITLE REPORT PARCEL DESCRIPTION(PER TITLE REPORT)**

All that certain tract or parcel of land, with all improvements thereon and appurtenances thereunto belonging, situate, lying and being in the Shawsville Magisterial of Montgomery County, Virginia, on the Easterly side of Virginia State Route No. 615, more particularly bounded and described as follows, to-wit:

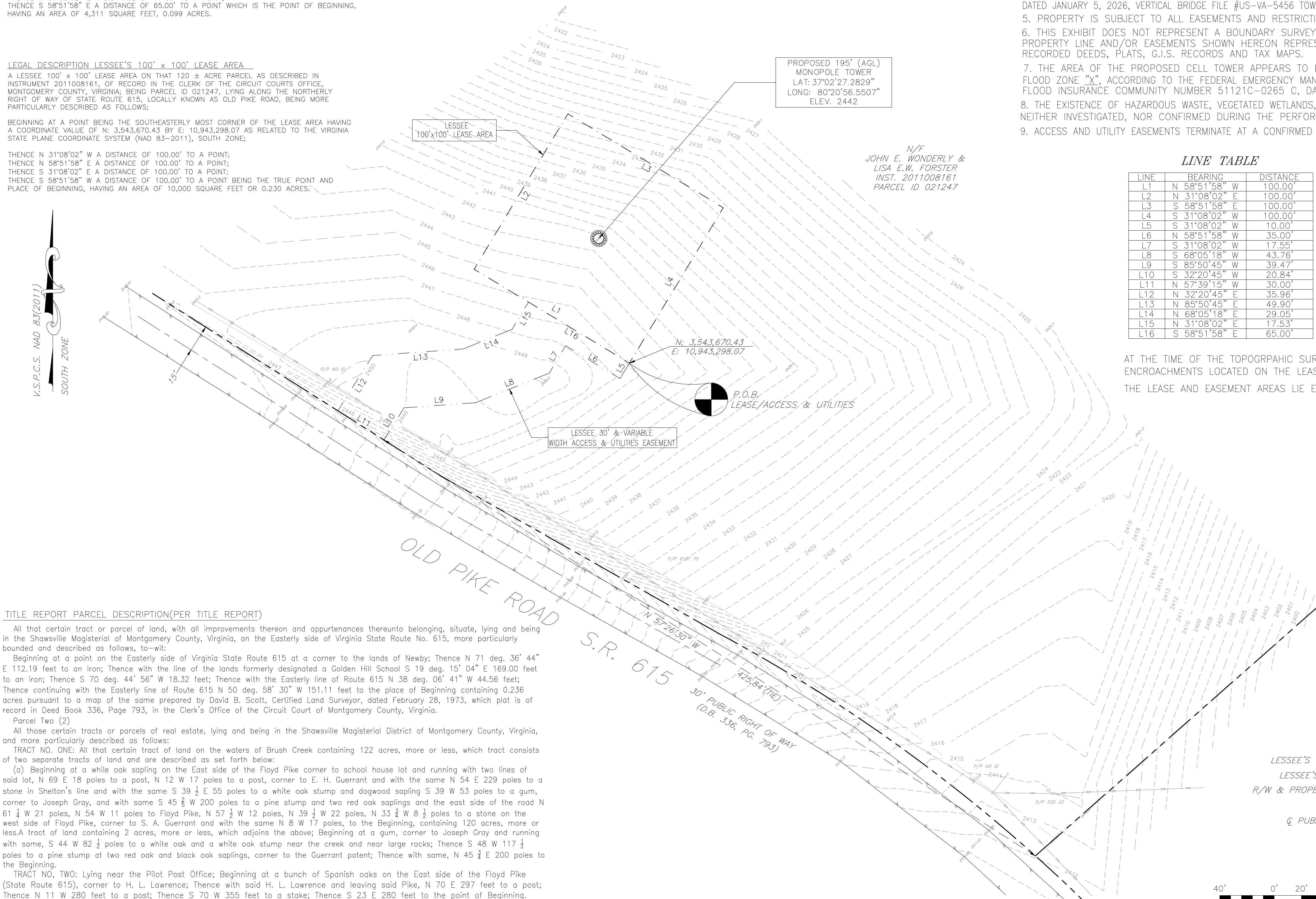
Beginning at a point on the Easterly side of Virginia State Route 615 at a corner to the lands of Newby; Thence N 71 deg. 36' 44" E 112.19 feet to an iron; Thence with the line of the lands formerly designated a Golden Hill School S 19 deg. 15' 04" E 169.00 feet to an iron; Thence S 70 deg. 44' 56" W 18.32 feet; Thence with the Easterly line of Route 615 N 38 deg. 06' 41" W 44.56 feet; Thence continuing with the Easterly line of Route 615 N 50 deg. 58' 30" W 151.11 feet to the place of Beginning containing 0.236 acres pursuant to a map of the same prepared by David B. Scott, Certified Land Surveyor, dated February 28, 1973, which plat is of record in Deed Book 336, Page 793, in the Clerk's Office of the Circuit Court of Montgomery County, Virginia.

Parcel Two (2)  
 All those certain tracts or parcels of real estate, lying and being in the Shawsville Magisterial District of Montgomery County, Virginia, and more particularly described as follows:

TRACT NO. ONE: All that certain tract of land on the waters of Brush Creek containing 122 acres, more or less, which tract consists of two separate tracts of land and are described as set forth below:

(a) Beginning at a white oak sapling on the East side of the Floyd Pike corner to school house lot and running with two lines of said lot, N 69 E 18 poles to a post, N 12 W 17 poles to a post, corner to E. H. Guerrant and with the same N 54 E 229 poles to a stone in Shelton's line and with the same S 39 1/2 E 55 poles to a white oak stump and dogwood sapling S 39 W 53 poles to a gum, corner to Joseph Gray, and with some S 45 1/2 W 200 poles to a pine stump and two red oak saplings and the east side of the road N 61 1/2 W 21 poles, N 54 W 11 poles to Floyd Pike, N 57 1/2 W 12 poles, N 39 1/2 W 22 poles, N 33 3/4 W 8 1/2 poles to a stone on the west side of Floyd Pike, corner to S. A. Guerrant and with the same N 8 W 17 poles, to the Beginning, containing 120 acres, more or less. A tract of land containing 2 acres, more or less, which adjoins the above; Beginning at a gum, corner to Joseph Gray and running with same, S 44 W 82 1/2 poles to a white oak and a white oak stump near the creek and near large rocks; Thence S 48 W 117 1/2 poles to a pine stump at two red oak and black oak saplings, corner to the Guerrant patent; Thence with same, N 45 3/4 E 200 poles to the Beginning.

TRACT NO. TWO: Lying near the Pilot Post Office; Beginning at a bunch of Spanish oaks on the East side of the Floyd Pike (State Route 615), corner to H. L. Lawrence; Thence with said H. L. Lawrence and leaving said Pike, N 70 E 297 feet to a post; Thence N 11 W 280 feet to a post; Thence S 70 W 355 feet to a stake; Thence S 23 E 280 feet to the point of Beginning.  
 Parcel ID: 021247

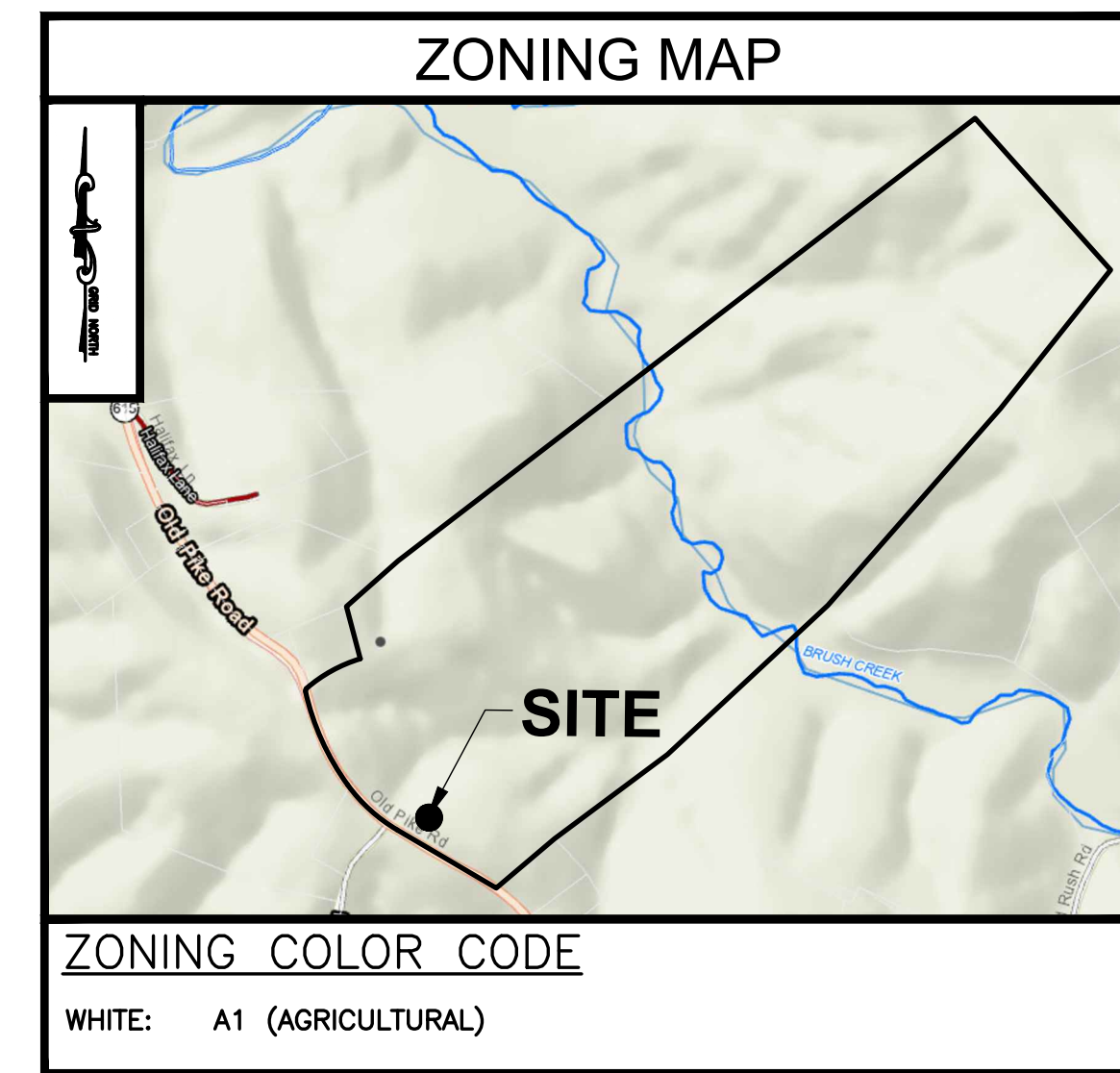
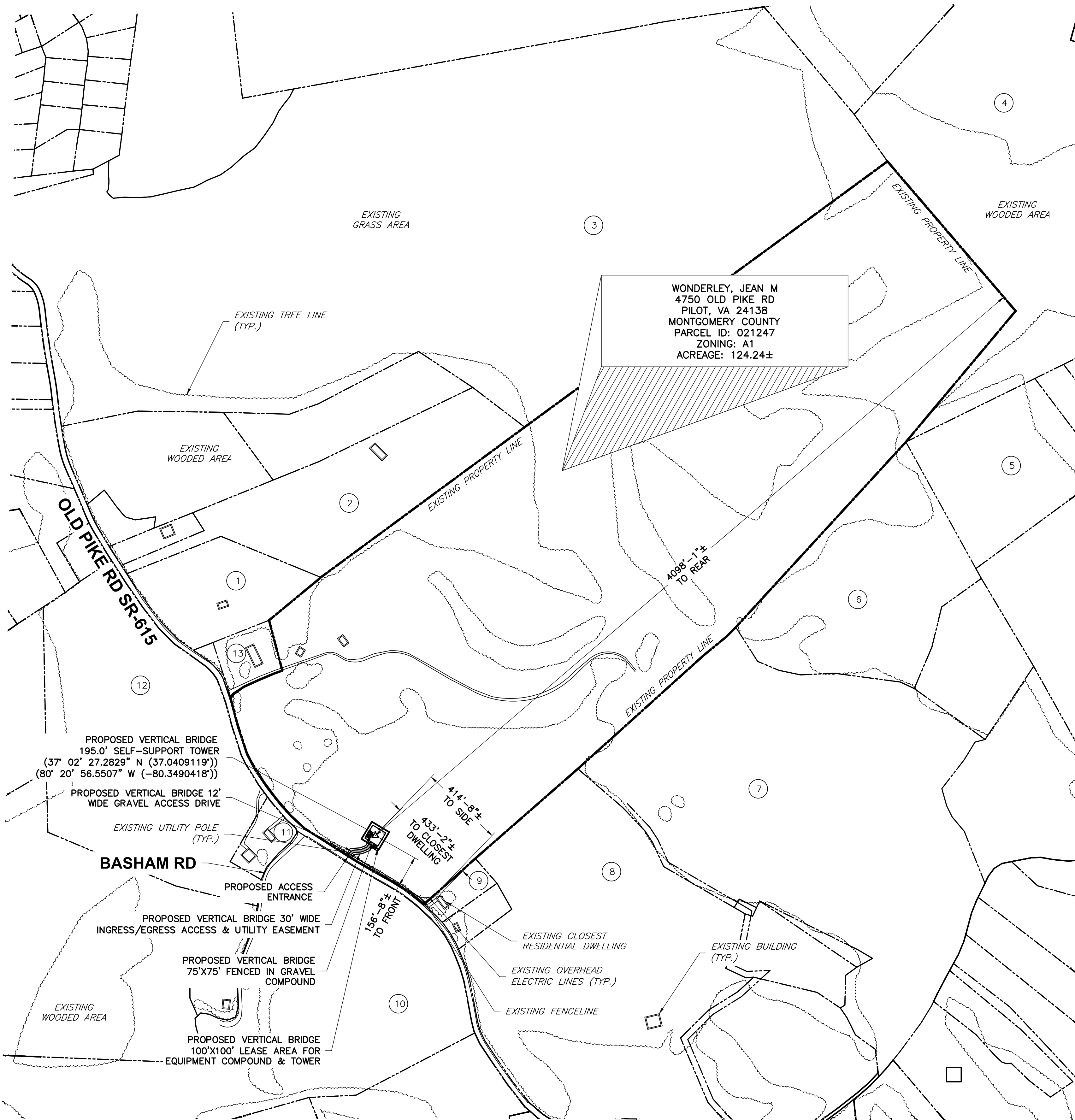


- LEGEND**
- LESSEE'S PREMISES ————
  - LESSEE'S ACCESS ————
  - R/W & PROPERTY LINE ————
  - TIE LINE - - - - -
  - Q PUBLIC ROAD - - - - -

GRAPHIC SCALE  
 1"=40'(22x34)  
 1"=80'(11x17)



APPLICANT	 PREPARED FOR THE TOWERS																
ENGINEER	 <b>TOTALLY COMMITTED.</b>  <b>NB+C ENGINEERING SERVICES, LLC.</b> <small>120 EASTSHORE DRIVE, SUITE 300          GLEN ALLEN, VA 23059          804-545-4078</small>																
SITE INFORMATION	<b>VERTICAL BRIDGE EASEMENT EXHIBIT</b> SITE NAME: OLD PIKE SITE # US-VA-5456 #4752 OLD PIKE ROAD PILOT, VA 24138 MONTGOMERY COUNTY																
DESIGN RECORD	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th colspan="4" style="text-align: center;">REVISIONS</th> </tr> <tr> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> </tr> </thead> <tbody> <tr> <td style="text-align: center;">1</td> <td style="text-align: center;">4/27/26</td> <td style="text-align: center;">REV VB COMM</td> <td style="text-align: center;">RTW</td> </tr> <tr> <td style="text-align: center;">0</td> <td style="text-align: center;">3/31/26</td> <td style="text-align: center;">EXHIBIT</td> <td style="text-align: center;">RTW</td> </tr> </tbody> </table>	REVISIONS				REV	DATE	DESCRIPTION	BY	1	4/27/26	REV VB COMM	RTW	0	3/31/26	EXHIBIT	RTW
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REV	DATE	DESCRIPTION	BY														
1	4/27/26	REV VB COMM	RTW														
0	3/31/26	EXHIBIT	RTW														
SHEET TITLE	  <b>EASEMENT EXHIBIT</b>																
SHEET NUMBER	  <b>EE-1</b>																



**GENERAL NOTES**

1. THIS PLAN IS SUBJECT TO ALL EASEMENTS AND RESTRICTIONS OF RECORD.
2. NO SIGNIFICANT NOISE, SMOKE, DUST, OR ODOR WILL RESULT FROM THIS FACILITY.
3. THE FACILITY IS UNMANNED AND NOT INTENDED FOR HUMAN HABITATION. THERE IS NO HANDICAP ACCESS REQUIRED.
4. THE FACILITY IS UNMANNED AND DOES NOT REQUIRE POTABLE WATER OR SANITARY SERVICE.

**LEGEND**

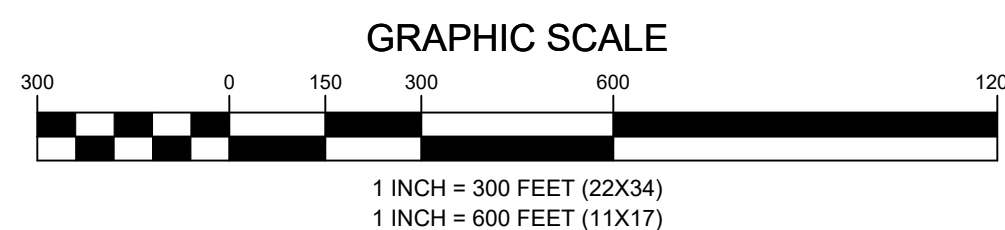
	PROPERTY LINE - SUBJECT PARCEL
	PROPERTY LINE - ABUTTERS
	EXISTING ROAD
	EXISTING FENCE
	PROPOSED EASEMENT
	PROPOSED LEASE AREA
	EXISTING EASEMENT
	PROPOSED FENCE
	EXISTING OVERHEAD UTILITY LINES
	EXISTING BUILDING

**ZONING INFORMATION**

JURISDICTION: MONTGOMERY COUNTY		
ZONING: A1		
DIMENSION	PROPOSED ±	REQUIRED
FRONT SETBACK:	156'-8"±	100'-0"
SIDE SETBACK:	414'-8"±	100'-0"
REAR SETBACK:	4098'-1"±	100'-0"
CLOSEST DWELLING:	433'-2"±	-
LOT AREA: 124.24 ± ACRES		
(ALL MEASUREMENTS ARE IN FEET ± UNLESS OTHERWISE NOTED)		



**1 SITE PLAN**  
SCALE: 1" = 300' (22X34)  
SCALE: 1" = 600' (11X17)



ENGINEERING FIRM	<b>NB+C</b> TOTALLY COMMITTED. NB+C ENGINEERING SERVICES, LLC. 120 EASTSHORE DRIVE, SUITE 300 GLEN ALLEN, VA 23059 (804) 548-4079
APPLICANT	<b>verticalbridge</b> THE TOWERS, LLC
CARRIER	<b>verizon</b>
SITE INFORMATION	OLD PIKE VERTICAL BRIDGE RAWLAND SITE #: US-VA-5456 NB+C PROJECT #: 100912 4752 OLD PIKE ROAD PILOT, VA 24138 MONTGOMERY COUNTY
DESIGN RECORD	<b>REVISIONS</b>
PROFESSIONAL STAMP	
ENGINEER	JOHN A. DAUGHTREY III, P.E. VIRGINIA PROFESSIONAL ENGINEER LICENSE #0402052122
SHEET TITLE	<b>SITE PLAN</b>
SHEET NUMBER	<b>Z-1</b>

**ENGINEERING FIRM**



**TOTALLY COMMITTED.**

**NB+C ENGINEERING SERVICES, LLC.**  
 120 EASTSHORE DRIVE, SUITE 300  
 GLEN ALLEN, VA 23059  
 (804) 548-4079

**APPLICANT**



**THE TOWERS, LLC**

**CARRIER**



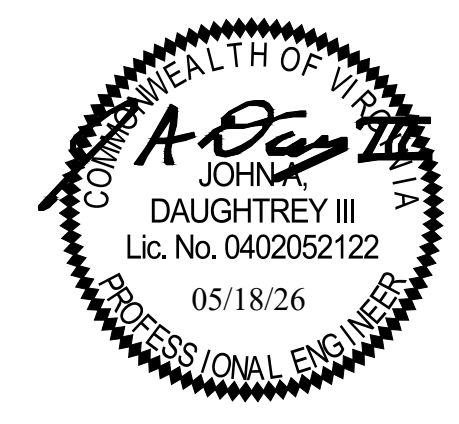
**SITE INFORMATION**

OLD PIKE  
 VERTICAL BRIDGE RAWLAND  
 SITE #: US-VA-5456  
 NB+C PROJECT #: 100912  
 4752 OLD PIKE ROAD  
 PILOT, VA 24138  
 MONTGOMERY COUNTY

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A	04/09/26	PRELIMINARY ZDS	SK

**PROFESSIONAL STAMP**



**ENGINEER**

JOHN A. DAUGHTREY III, P.E.  
 VIRGINIA PROFESSIONAL ENGINEER  
 LICENSE #0402052122

**SHEET TITLE**

**ADJACENT  
 PROPERTY  
 INFO**

**SHEET NUMBER**

**Z-2**

①  
 BRADLEY JEREMY S  
 4712 OLD PIKE RD  
 PILOT, VA 24138  
 PARCEL ID: 024115  
 ZONING: A1  
 AREA: 6.49± ACRES

②  
 ANDERSON ALLEN ARTHUR,  
 ANDERSON KATHLEEN CAUGHY  
 4670 OLD PIKE RD  
 PILOT, VA 24138  
 PARCEL ID: 013477  
 ZONING: A1  
 AREA: 13.749± ACRES

③  
 DEHART MOYER TURNER JR  
 4531 OLD PIKE RD  
 PILOT, VA 24138  
 PARCEL ID: 004875  
 ZONING: A1  
 AREA: 132.62± ACRES

④  
 WOLF RANDY R, WOLF JOAN H  
 4638 GOLD RUSH RD  
 PILOT, VA 24138  
 PARCEL ID: 002751  
 ZONING: A1  
 AREA: 74.0± ACRES

⑤  
 JESSIE RUBY JANICE  
 5006 GOLD RUSH RD PILOT,  
 VA 24138  
 PARCEL ID: 100569  
 ZONING: A1  
 AREA: 15.597± ACRES

⑥  
 JESSIE RUBY JANICE  
 5006 GOLD RUSH RD  
 PILOT, VA 24138  
 PARCEL ID: 013377  
 ZONING: A1  
 AREA: 18.0± ACRES

⑦  
 JESSIE RUBY JANICE  
 5006 GOLD RUSH RD  
 PILOT, VA 24138  
 PARCEL ID: 016405  
 ZONING: A1  
 AREA: 50.194± ACRES

⑧  
 NAFF TIMOTHY ERIC  
 PO BOX 48  
 PILOT, VA 24138  
 PARCEL ID: 013378  
 ZONING: A1  
 AREA: 36.903± ACRES

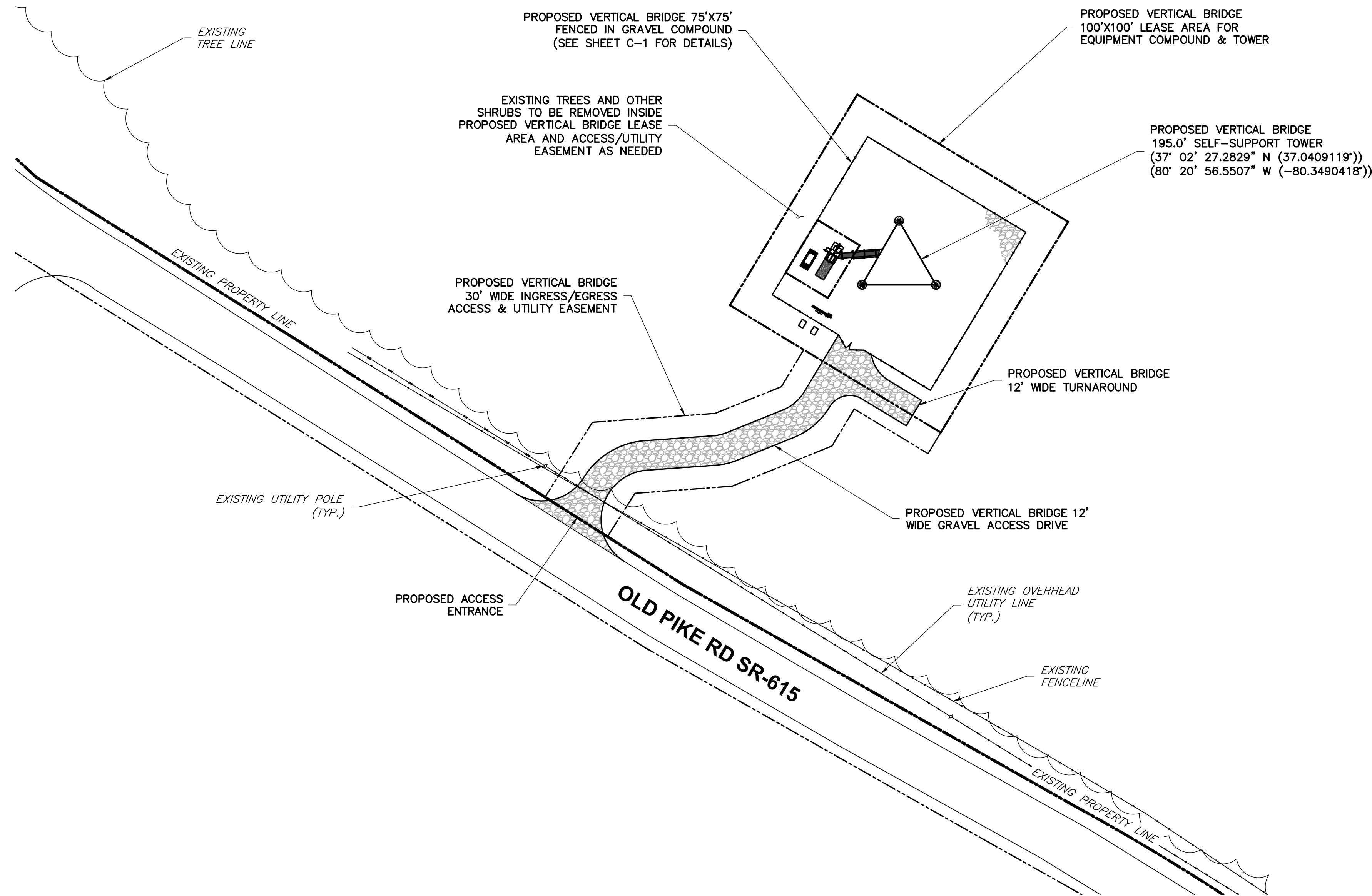
⑨  
 BOWER LARRY ALLEN  
 4874 OLD PIKE RD  
 PILOT, VA 24138  
 PARCEL ID: 009075  
 ZONING: A1  
 AREA: 0.438± ACRES

⑩  
 BOOTHE ERNEST H JR,  
 BOOTHE SHARON E  
 4809 OLD PIKE RD  
 PILOT, VA 24138  
 PARCEL ID: 002165  
 ZONING: A1  
 AREA: 58.80± ACRES

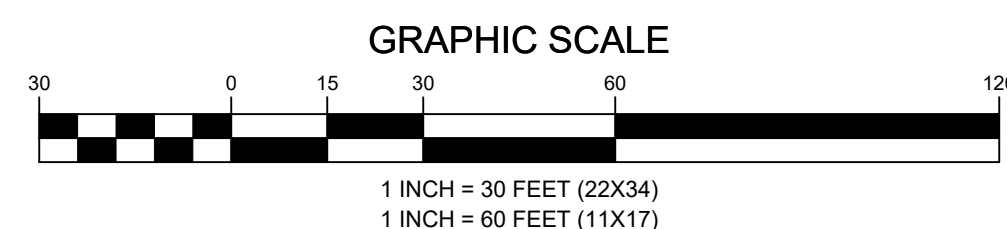
⑪  
 BOOTHE ERNEST HARVEY  
 JR/BOOTHE SHARON EANES  
 4809 OLD PIKE RD  
 PILOT, VA 24138  
 PARCEL ID: 002180  
 ZONING: A1  
 AREA: 1.41± ACRES

⑫  
 BOOTHE ERNEST H JR /  
 BOOTHE SHARON E  
 4809 OLD PIKE RD  
 PILOT, VA 24138  
 PARCEL ID: 002164  
 ZONING: A1  
 AREA: 95.0± ACRES

⑬  
 WONDERLEY JOHN L,  
 WONDERLEY JEAN M  
 4750 OLD PIKE RD  
 PILOT, VA 24138  
 PARCEL ID: 021247  
 ZONING: A1  
 AREA: 124.24± ACRES



1  
Z-3  
**ENLARGED SITE PLAN**  
SCALE: 1" = 30' (22X34)  
SCALE: 1" = 60' (11X17)



Know what's below.  
Call before you dig.

ENGINEERING FIRM

**NB+C**  
TOTALLY COMMITTED.  
NB+C ENGINEERING SERVICES, LLC.  
120 EASTSHORE DRIVE, SUITE 300  
GLEN ALLEN, VA 23059  
(804) 548-4079

APPLICANT

**verticalbridge**  
THE TOWERS, LLC

CARRIER

**verizon**

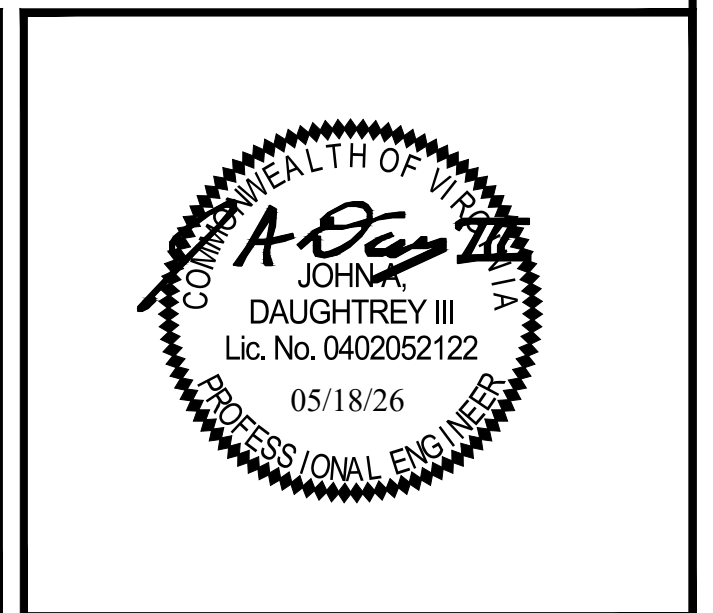
SITE INFORMATION

OLD PIKE  
VERTICAL BRIDGE RAWLAND  
SITE #: US-VA-5456  
NB+C PROJECT #: 100912  
4752 OLD PIKE ROAD  
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MONTGOMERY COUNTY

DESIGN RECORD

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PROFESSIONAL STAMP



ENGINEER

JOHN A. DAUGHTREY III, P.E.  
VIRGINIA PROFESSIONAL ENGINEER  
LICENSE #0402052122

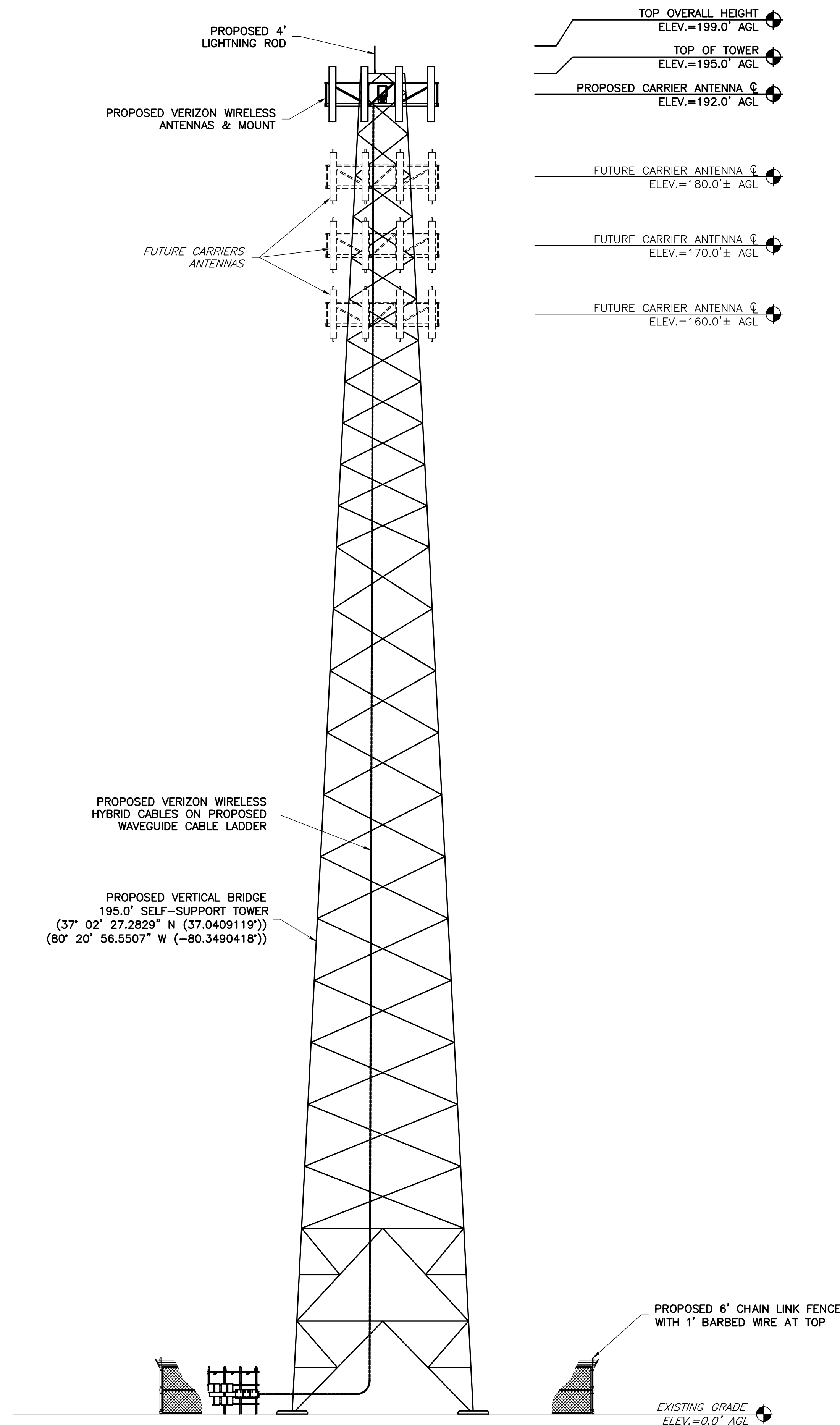
SHEET TITLE

**ENLARGED  
SITE PLAN**

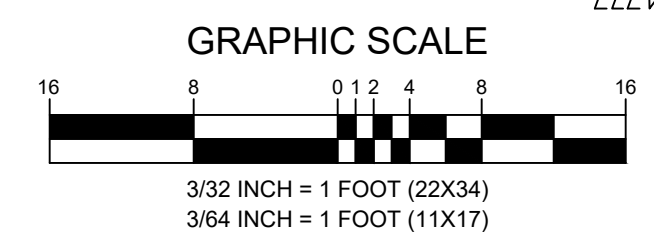
SHEET NUMBER

**Z-3**





1 ELEVATION  
C-2  
SCALE: 3/32" = 1' (22X34)  
SCALE: 3/64" = 1' (11X17)



**GENERAL NOTES**

1. THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE CODES ORDINANCES, LAWS AND REGULATIONS OF ALL MUNICIPALITIES, UTILITIES COMPANY OR OTHER PUBLIC AUTHORITIES.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS AND INSPECTIONS THAT MAY BE REQUIRED BY ANY FEDERAL, STATE, COUNTY OR MUNICIPAL AUTHORITIES.
3. THE CONTRACTOR SHALL NOTIFY THE CONSTRUCTION MANAGER, IN WRITING, OF ANY CONFLICTS, ERRORS OR OMISSIONS PRIOR TO THE SUBMISSION OF BIDS OR PERFORMANCE OF WORK. MINOR OMISSIONS OR ERRORS IN THE BID DOCUMENTS SHALL NOT RELIEVE THE CONTRACTOR FROM RESPONSIBILITY FOR THE OVERALL INTENT OF THESE DRAWINGS.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL EXISTING SITE IMPROVEMENTS PRIOR TO COMMENCING CONSTRUCTION. THE CONTRACTOR SHALL REPAIR ANY DAMAGE CAUSED AS A RESULT OF CONSTRUCTION OF THIS FACILITY.
5. THE SCOPE OF WORK FOR THIS PROJECT SHALL INCLUDE PROVIDING ALL MATERIALS, EQUIPMENT AND LABOR REQUIRED TO COMPLETE THIS PROJECT. ALL EQUIPMENT SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
6. THE CONTRACTOR SHALL VISIT THE PROJECT SITE PRIOR TO SUBMITTING A BID TO VERIFY THAT THE PROJECT CAN BE CONSTRUCTED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
7. CONTRACTOR SHALL VERIFY ANTENNA ELEVATION AND AZIMUTH WITH RF ENGINEERING PRIOR TO INSTALLATION.
8. TRANSMITTER EQUIPMENT AND ANTENNAS ARE DESIGNED TO MEET ANSI/TIA 222-H REQUIREMENTS.
9. ALL STRUCTURAL ELEMENTS SHALL BE HOT DIPPED GALVANIZED STEEL.
10. CONTRACTOR SHALL MAKE A UTILITY "ONE CALL" TO LOCATE ALL UTILITIES PRIOR TO EXCAVATING.
11. IF ANY UNDERGROUND UTILITIES OR STRUCTURES EXIST BENEATH THE PROJECT AREA, CONTRACTOR MUST LOCATE IT AND CONTACT THE APPLICANT & THE OWNER'S REPRESENTATIVE.
12. OCCUPANCY IS LIMITED TO PERIODIC MAINTENANCE AND INSPECTION BY TECHNICIANS APPROXIMATELY 2 TIMES PER MONTH.
13. PRIOR TO THE INSTALLATION OF THE PROPOSED EQUIPMENT OR MODIFICATION OF THE EXISTING STRUCTURE, A STRUCTURAL ANALYSIS SHALL BE PERFORMED BY THE OWNER'S AGENT TO CERTIFY THAT THE EXISTING/PROPOSED COMMUNICATION STRUCTURE AND COMPONENTS ARE STRUCTURALLY ADEQUATE TO SUPPORT ALL EXISTING AND PROPOSED ANTENNAS, COAXIAL CABLES AND OTHER APPURTENANCES.
14. PROPERTY LINE INFORMATION WAS PREPARED USING DEEDS, TAX MAPS, AND PLANS OF RECORD AND SHOULD NOT BE CONSTRUED AS AN ACCURATE BOUNDARY SURVEY.
15. THIS PLAN IS SUBJECT TO ALL EASEMENTS AND RESTRICTIONS OF RECORD.
16. THE PROPOSED FACILITY WILL CAUSE ONLY A "DE MINIMIS" INCREASE IN STORMWATER RUNOFF. THEREFORE, NO DRAINAGE STRUCTURES ARE PROPOSED.
17. NO SIGNIFICANT NOISE, SMOKE, DUST, OR ODOR WILL RESULT FROM THIS FACILITY.
18. THE FACILITY IS UNMANNED AND NOT INTENDED FOR HUMAN HABITATION (NO HANDICAP ACCESS REQUIRED).
19. THE FACILITY IS UNMANNED AND DOES NOT REQUIRE POTABLE WATER OR SANITARY SERVICE.
20. POWER TO THE FACILITY WILL BE MONITORED BY A SEPARATE METER.

**NOTE:**  
THE TOWER SHALL BE MAINTAINED WITH A GALVANIZED STEEL FINISH OR BE PAINTED A NEUTRAL COLOR. DISH ANTENNAS SHALL BE A NEUTRAL, NONREFLECTIVE COLOR. NO LOGOS OR ADVERTISING OF ANY KIND SHALL BE PERMITTED ON THE TOWER, ANTENNAS OR ANY ACCOMPANYING STRUCTURES OR FACILITIES.

**NOTE:**  
STRUCTURAL ANALYSIS OF STRUCTURE PERFORMED INDEPENDENT FROM THESE DRAWINGS.

**ENGINEERING FIRM**

**TOTALLY COMMITTED.**

NB+C ENGINEERING SERVICES, LLC.  
120 EASTSHORE DRIVE, SUITE 300  
GLEN ALLEN, VA 23059  
(804) 548-4079

**APPLICANT**

verticalbridge  
THE TOWERS, LLC

**CARRIER**

**SITE INFORMATION**

OLD PIKE  
VERTICAL BRIDGE RAWLAND  
SITE #: US-VA-5456  
NB+C PROJECT #: 100912  
4752 OLD PIKE ROAD  
PILOT, VA 24138  
MONTGOMERY COUNTY

**DESIGN RECORD**

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1	05/05/26	REVISED FINAL ZDS	JC
0	04/09/26	FINAL ZDS	SK
A	04/09/26	PRELIMINARY ZDS	SK

**PROFESSIONAL STAMP**

**ENGINEER**

JOHN A. DAUGHTREY III, P.E.  
VIRGINIA PROFESSIONAL ENGINEER  
LICENSE #0402052122

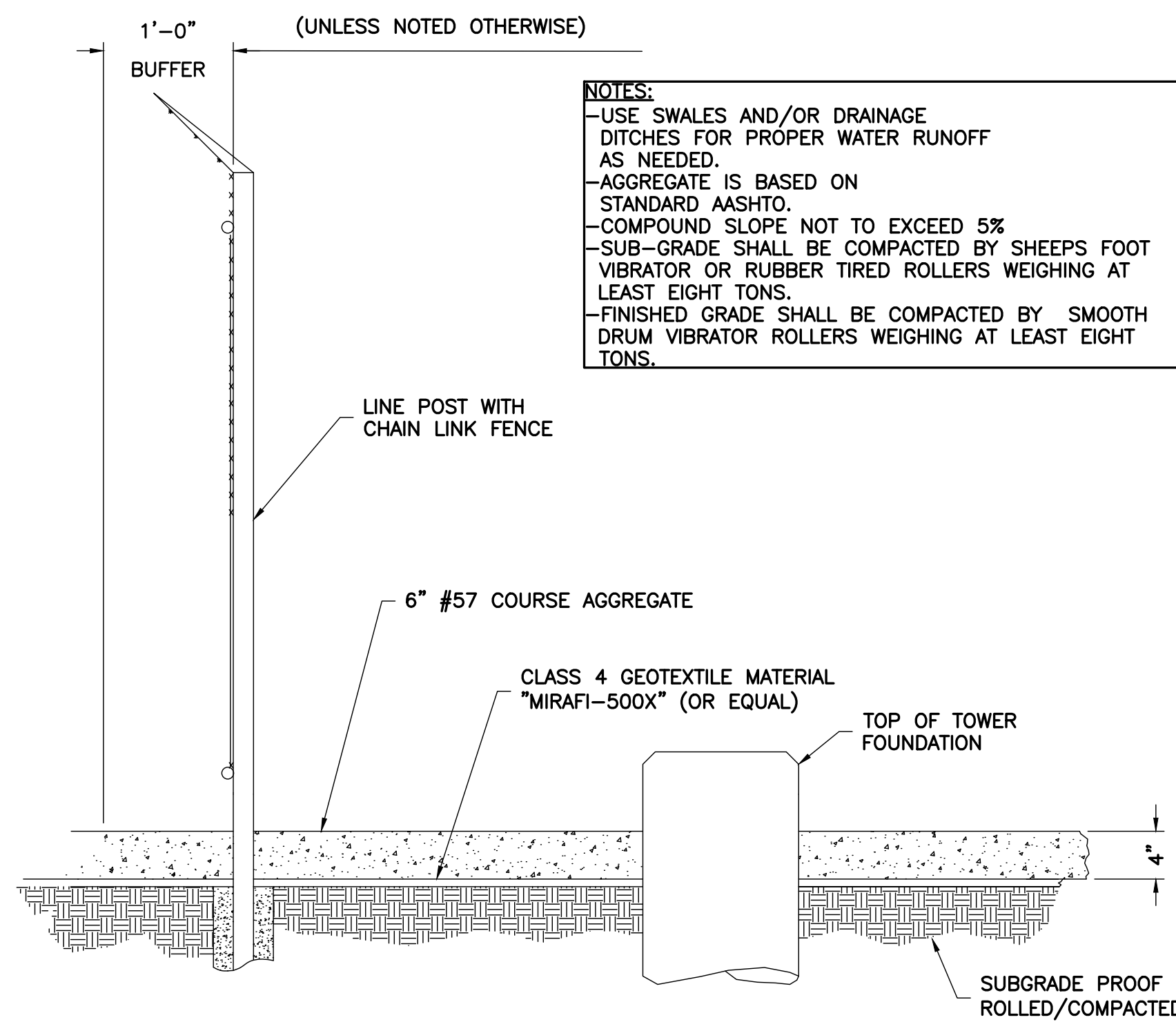
**SHEET TITLE**

**TOWER ELEVATION & NOTES**

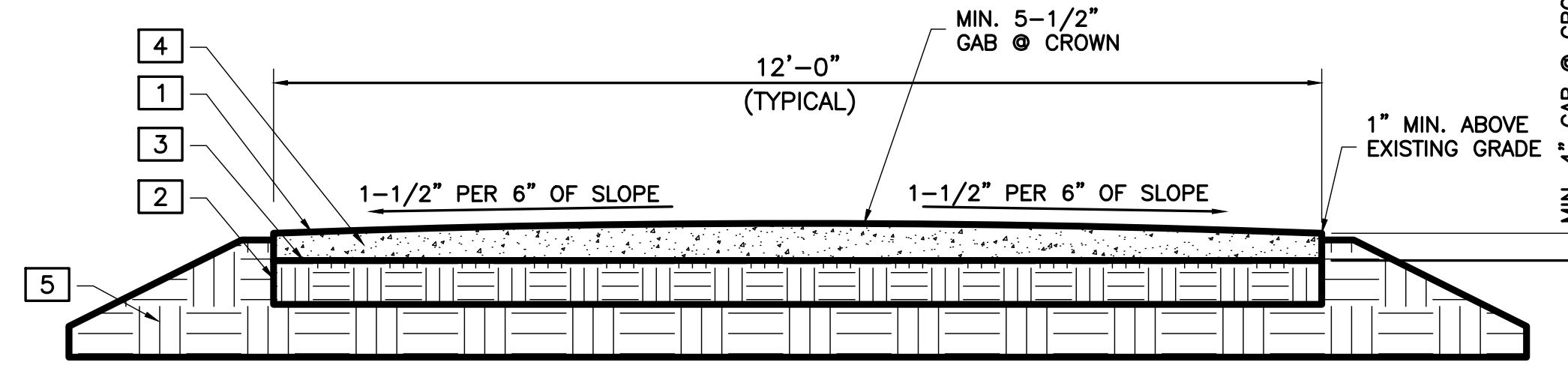
**SHEET NUMBER**

**C-2**





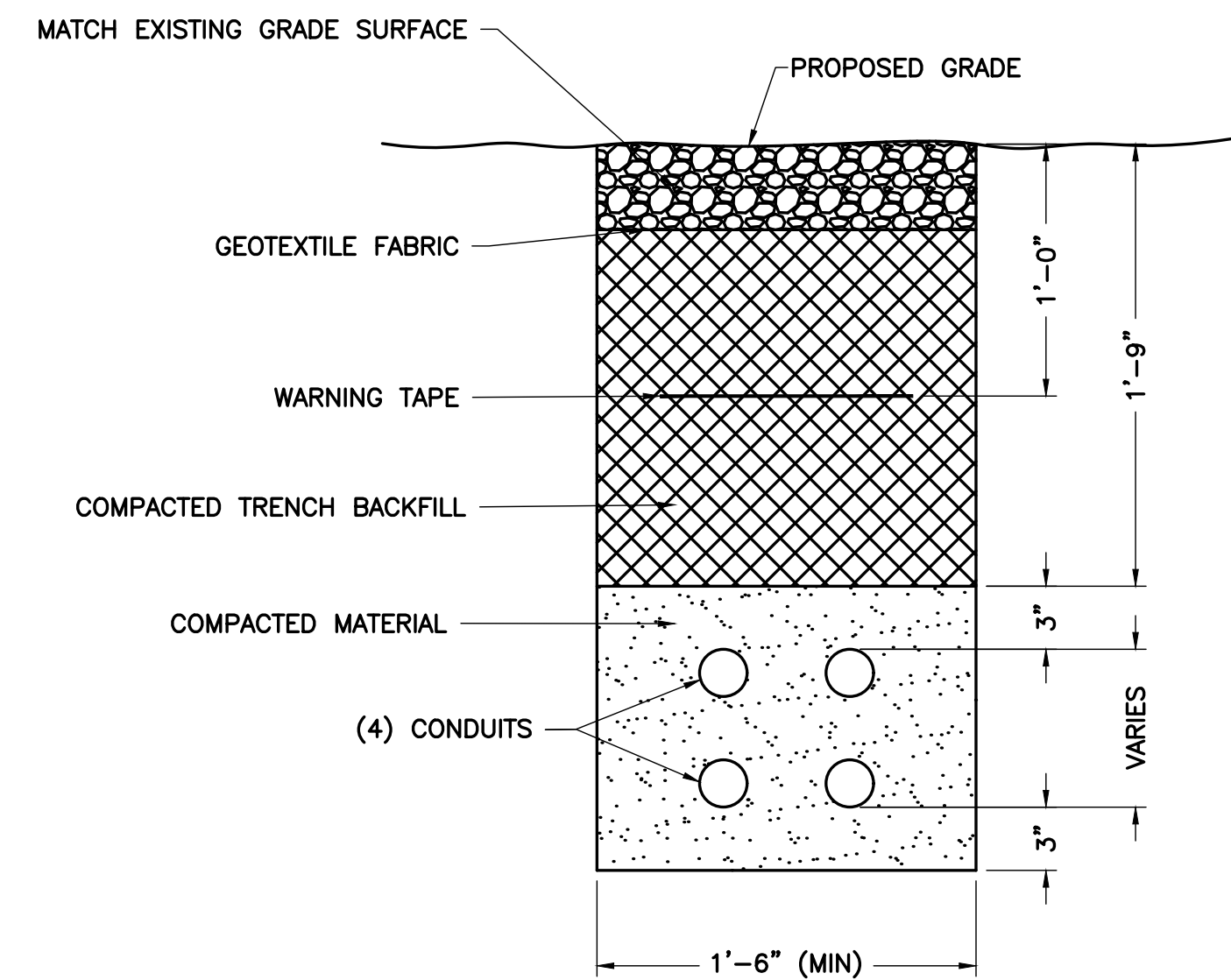
1 SITE COMPOUND SURFACING DETAIL  
S-1 NTS



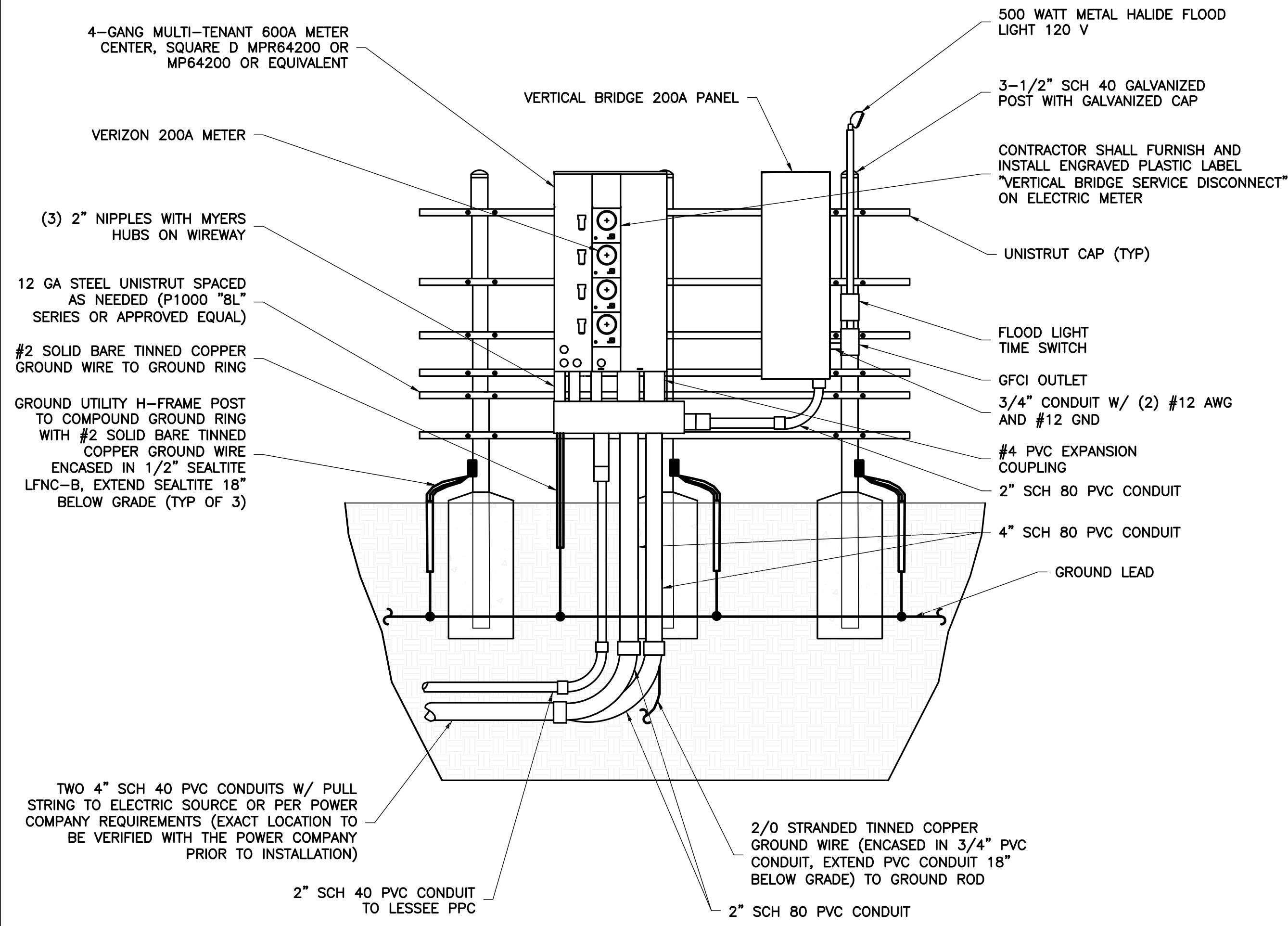
TYPICAL GRAVEL ROAD CROSS SECTION:  
\*\*CONTRACTOR SHALL BE RESPONSIBLE FOR CLEARS \* GRUBBING THE CONSTRUCTION SITE AND ROADWAY AREAS. THE CONTRACTOR SHALL COMPLY, WHEN NECESSARY, WITH THE RECOMMENDATIONS CONTAINED WITHIN THE GEOTECHNICAL REPORT PREPARED FOR THIS SITE FOR SITE WORK PREPARATION & FOUNDATION WORK.\*\*

1. MINIMUM 4" CRUSHER RUN FINISHED DRIVING SURFACE
2. MIRAFI 500X (OR EQUIVALENT) GEOFABRIC
3. 2" TO 3" MINIMUM CRUSHER RUN OVER THE SUB-BASE AND ROLLED
4. 3" #3 GRAVEL ROLLED SUB-BASE COURSE
5. SUBGRADE COMPACTED TO 95% STANDARD PROCTOR DENSITY

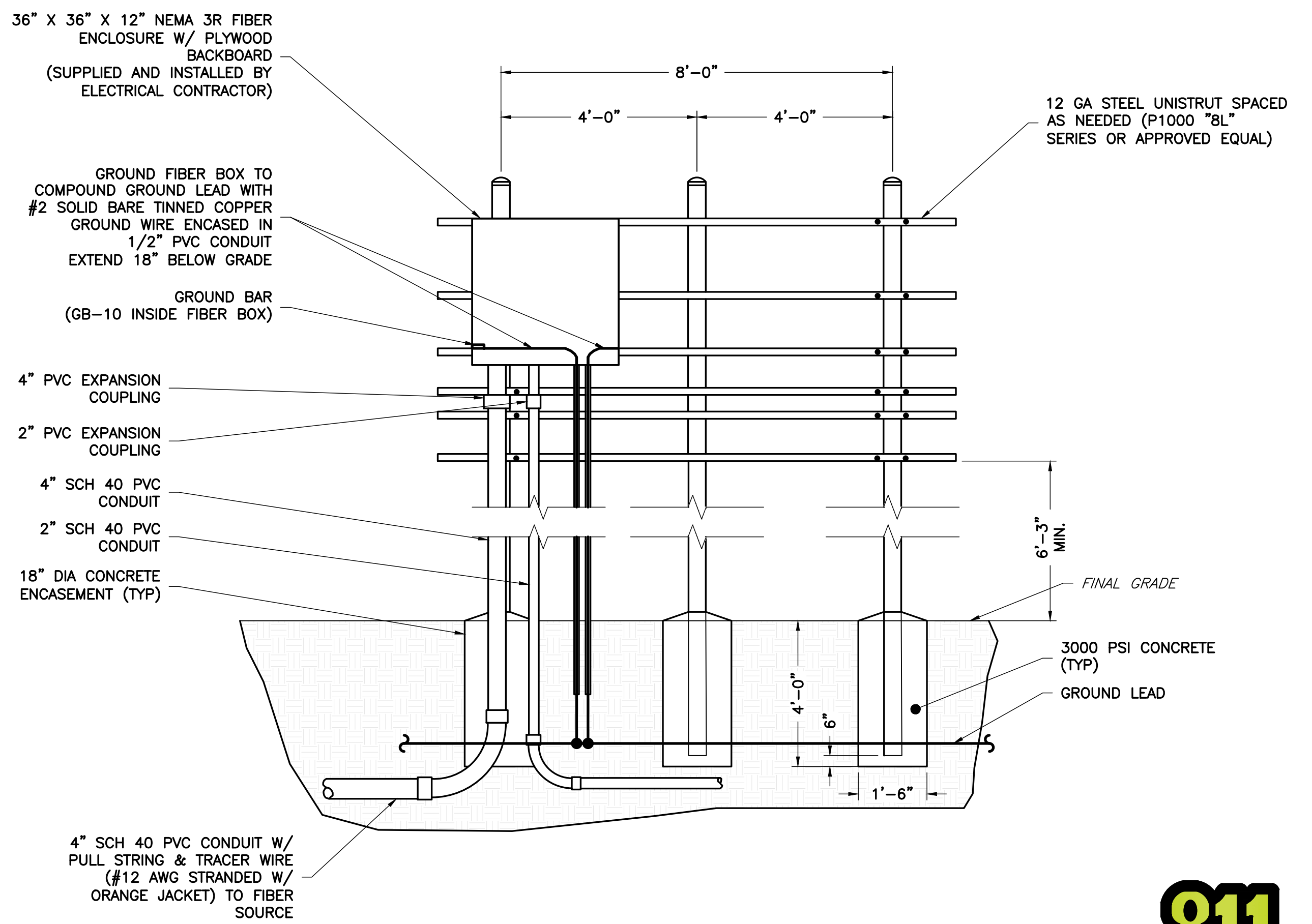
2 DRIVEWAY SECTION (GRAVEL)  
S-1 NTS



3 FABRIC/BAR CONNECTIONS  
S-1 NTS



4 H-FRAME FRONT VIEW - ELECTRIC  
S-1 NTS



5 H-FRAME REAR VIEW - FIBER  
S-1 NTS

**NB+C**  
TOTALLY COMMITTED.

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120 EASTSHORE DRIVE, SUITE 300  
GLEN ALLEN, VA 23059  
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PROFESSIONAL STAMP

JOHN A. DAUGHTREY III, P.E.  
VIRGINIA PROFESSIONAL ENGINEER  
LICENSE #0402052122

ENGINEER

JOHN A. DAUGHTREY III, P.E.  
VIRGINIA PROFESSIONAL ENGINEER  
LICENSE #0402052122

SHEET TITLE

CONSTRUCTION DETAILS

SHEET NUMBER

S-1



Know what's below.  
Call before you dig.

**REVISIONS**

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JOHN A. DAUGHTREY III, P.E.  
VIRGINIA PROFESSIONAL ENGINEER  
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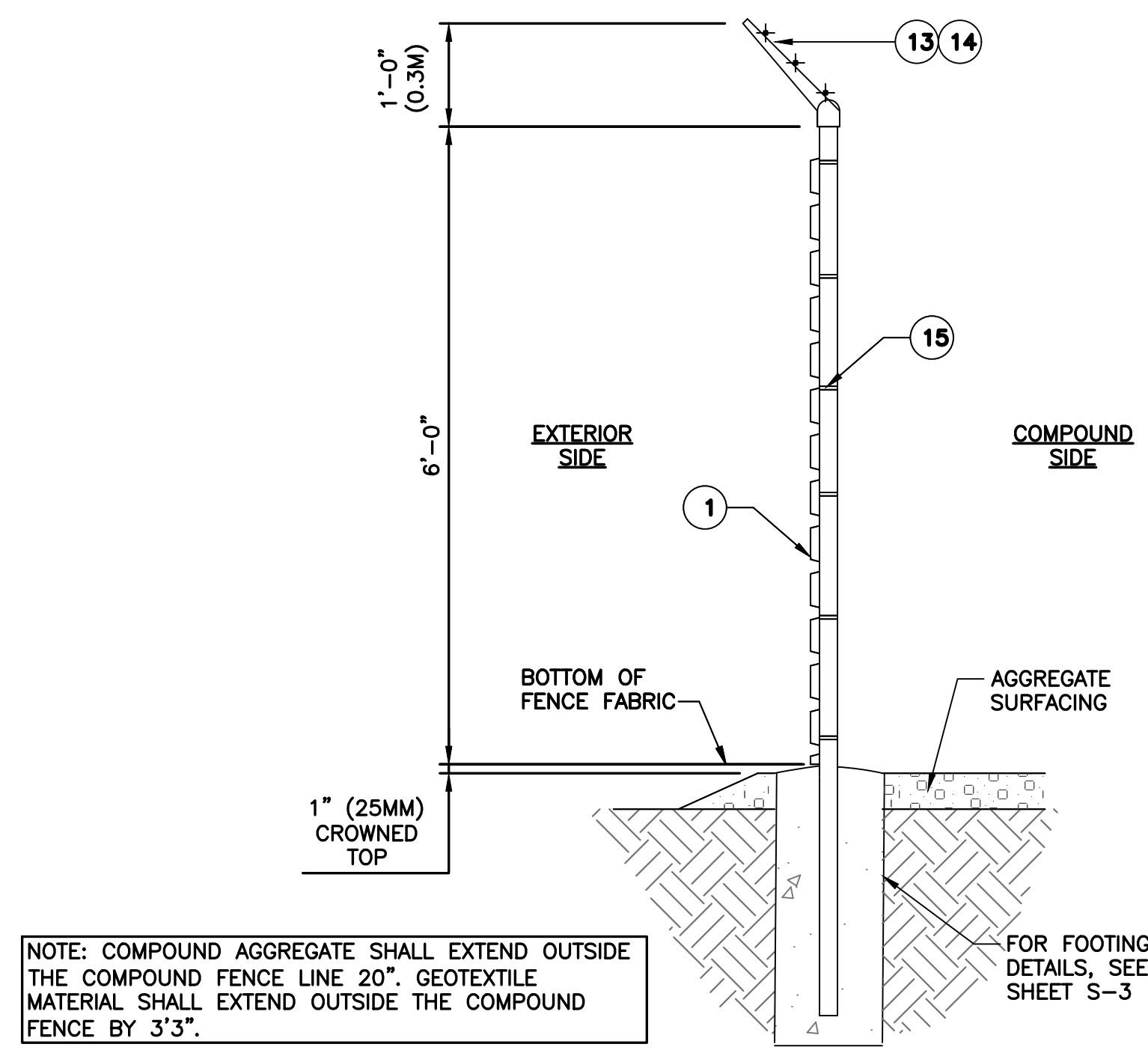
**FENCING  
DETAILS I**

**S-2**

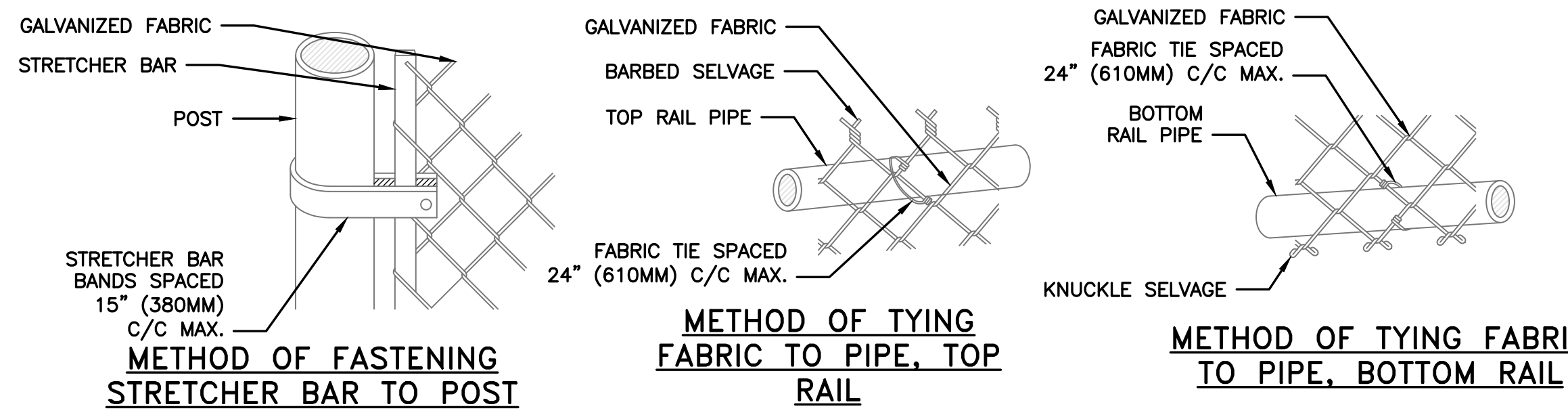
**MATERIAL SPECIFICATIONS:**

- ① CHAIN LINK COMMERCIAL FABRIC: 9-GAUGE WIRE MESH WITH 2.0" (50MM) MAX OPENING; GALVANIZED ASTM-A392, CLASS 2; TWISTED SELVAGE ON TOP, KNUCKLED ON BOTTOM.
- ② LINE POSTS: SEE TABLE ON THIS SHEET FOR SIZE.
- ③ PULL POSTS: 4.0" (100MM) O.D. PIPE, 0.226" (5.7MM) MINIMUM WALL THICKNESS.
- ④ CORNER POSTS: 4.0" (100MM) O.D. PIPE, 0.226" (5.7MM) MINIMUM WALL THICKNESS.
- ⑤ TOP RAIL: 1.66" (42MM) O.D. PIPE, 0.14" (3.5MM) MINIMUM WALL THICKNESS.
- ⑥ BRACE RAIL: 1.66" (42MM) O.D. PIPE, 0.14" (3.5MM) MINIMUM WALL THICKNESS.
- ⑦ DIAGONAL TRUSS ROD: 3/8" (9.5MM) GALVANIZED ROD WITH TURNBUCKLE.
- ⑧ TENSION BAR: 3/16" X 3/4" (4.8MM X 19MM) GALVANIZED FLAT BAR. SECURE BANDS WITH 5/16" (8MM) GALVANIZED STEEL CARRIAGE BOLTS.
- ⑨ BOTTOM RAIL: 1.66" (42MM) O.D. PIPE, 0.14" (3.5MM) MINIMUM WALL THICKNESS.
- ⑩ GATE POSTS: 4.0" (100MM) O.D. PIPE, 0.226" (5.7MM) MINIMUM WALL THICKNESS.
- ⑪ HIGH SECURITY COMBINATION PADLOCK. ABUS 190CS
- ⑫ GATE FRAMES: 1.9" (48MM) O.D. PIPE, 0.154" (4MM) MINIMUM WALL THICKNESS.
- ⑬ BARBED WIRE SUPPORT ARM: SINGLE ARM TYPE (GALVANIZED). ARM SHALL BE INCLINED OUTWARD AT AN ANGLE OF 45 DEGREES AND BE NO LESS THAN 18.0" (457MM) IN LENGTH. EXCEPTION: BARBED-WIRE TO BE ORIENTED VERTICALLY WHEN RUNNING ATOP GATES & DIRECTLY BEHIND LUMEN DEMARCATION AREA.
- ⑭ BARBED WIRE: GALVANIZED, ASTM A121 CLASS 3; THREE 14 GAUGE (2.0MM) MINIMUM STEEL WIRES WITH 4 POINT ROUND 14 GAUGE (2.0MM) BARBS.
- ⑮ FABRIC TIES: ALUMINUM BANDS OR WIRES. FABRIC SHALL BE ATTACHED TO THE TOP RAIL AND BOTTOM RAILS WITH TENSION WIRE AT 24" (610mm) CENTERS AND TO THE POSTS AT 15"
- ⑯ FOR COMPOUND VEHICLE GATE PROVIDE A STYMILOCK TO ALLOW PAD LOCK WITH 1/2"(13mm) SHACKLE TO BE INSTALLED ON OUTSIDE OF THE COMPOUND.

- ⑰ MISCELLANEOUS:
- A. RAIL COUPLINGS: SLEEVE TYPE, 152MM (6.0") LONG EXPANSION SPRING EVERY FIFTH COUPLING.
  - B. POST TOPS: PRESSED STEEL, MALLEABLE IRON WITH PRESSED STEEL EXTENSION ARM, OR ONE-PIECE ALUMINUM CASTING; WITH HOLE FOR TOP, ALL DESIGNED TO FIT OVER THE OUTSIDE OF THE POSTS AND TO PREVENT ENTRY OF MOISTURE INTO TUBULAR POSTS.
  - C. LATCHES SHALL BE ARRANGED FOR PADLOCKING WITH THE PADLOCK ACCESSIBLE FROM THE INSIDE OF THE GATE ONLY. LATCHES SHALL LIMIT GATE SEPARATION TO A MAXIMUM GAP OF 2" WHEN GATE IS LATCHED.
  - D. INSTALL FENCING PER ASTM-F567.
  - E. INSTALL SWING GATES PER ASTM-F900.
  - F. LOCAL ORDINANCE OF BARBED WIRE PERMIT REQUIREMENT SHALL BE COMPLETED IF REQUIRED.
  - G. ALL POSTS SHALL BE ASTM-F1083, SCHEDULE 40 REGULAR GRADE (30 KSI YIELD), OR APPROVED EQUAL



**1 CHAINLINK FENCE SECTION**  
S-2 NTS



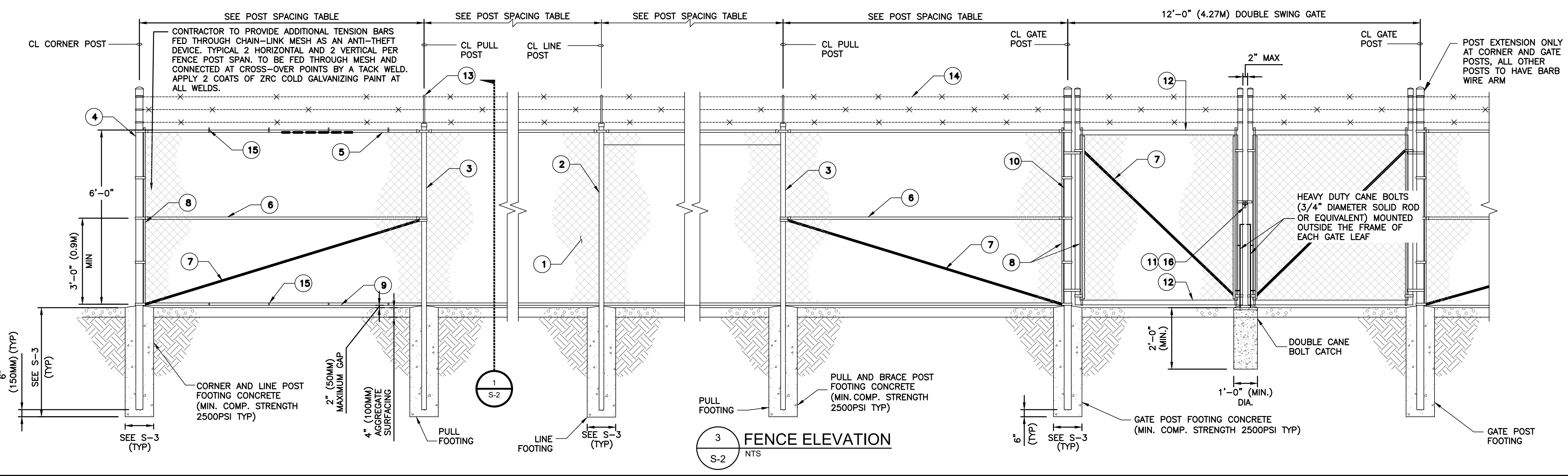
**2 FABRIC/BAR CONNECTIONS**  
S-2 NTS

**FENCE DESIGN CRITERIA STANDARD ASSUMPTIONS:**

- FENCE POST SIZES SHOWN IN THE TABLE BELOW ARE IN ACCORDANCE WITH CHAIN LINK FENCE WIND LOAD GUIDE WLG 2445 BY CHAIN LINK FENCE MANUFACTURER'S INSTITUTE (CLFM), UPDATED JANUARY, 2023.
- FOR SITE-SPECIFIC DESIGN, ENGINEER OF RECORD (EOR) SHALL REVIEW LOADING CRITERIA TO SELECT APPROPRIATE FENCE POST SIZE FROM THE TABLE BELOW. WHERE LOCAL DESIGN CODES AND PRACTICES REQUIRE, EOR SHALL DESIGN FENCE COMPONENTS AS APPROPRIATE.
- FENCE DESIGN STANDARDS APPLY TO CHAIN LINK FENCE FOR THE CONDITIONS NOTED BELOW. IT DOES NOT APPLY FOR FENCE WITH PRIVACY SLATS, RF FENCE, OR ANY OTHER FENCE VARIETIES.
- CHAIN LINK FABRIC: 9 GAUGE (3.7MM) WITH 2.0" (50MM) OPENING
- EXPOSURE CATEGORY: C (OPEN TERRAIN WITH SCATTERED OBSTRUCTIONS HAVING HEIGHTS GENERALLY LESS THAN 30 FT. THIS INCLUDES FLAT OPEN COUNTRY, GRASSLANDS, AND ALL WATER SURFACES IN HURRICANE PRONE REGIONS.
- ICE EXPOSURE REGIONAL CONDITIONS: REGION SUBJECT TO MODERATE ICING EFFECTS

**MINIMUM LINE POST SIZE AND SPACING FOR CHAIN LINK FENCE HEIGHT = 8'-0" (2.4M)**

DESIGN WIND SPEED	LINE POST SIZE DIA., IN (mm)	LINE POST WALL THICKNESS, IN (mm)	LINE POST SPACING, FT-IN (M)
> 110 MPH (177 KMPH), BUT ≤ 150 MPH (241 KMPH)	2.875" (73.03mm)	0.203" (5.16mm)	10'-0" (3.0 M)

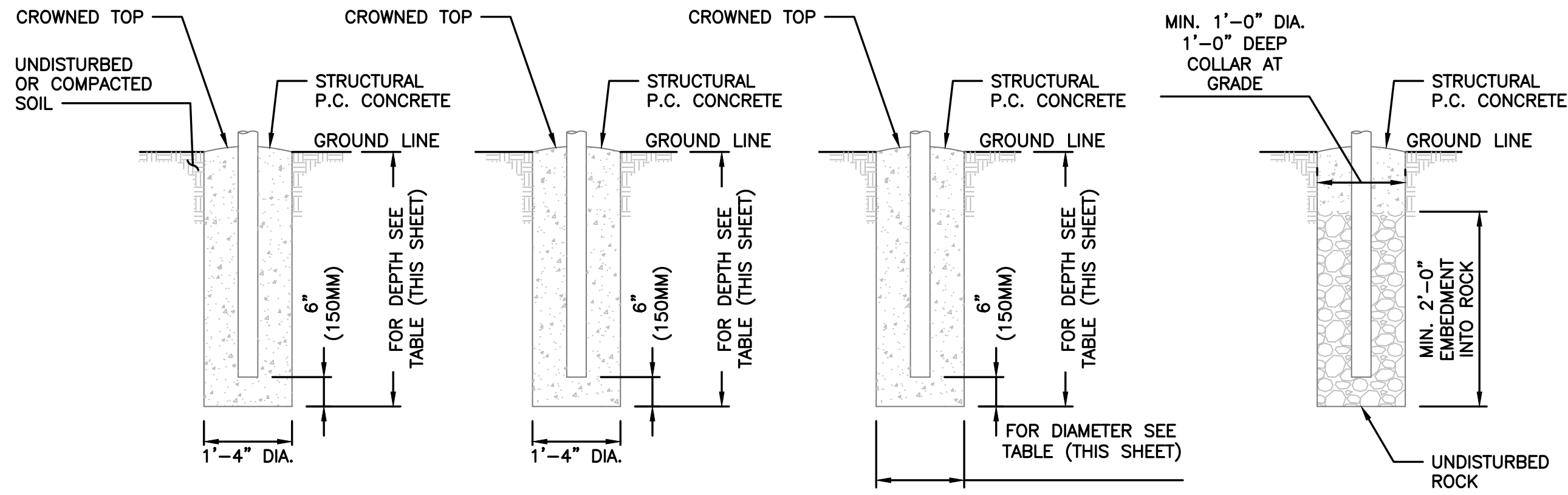


**3 FENCE ELEVATION**  
S-2 NTS

8'-0" HIGH FENCE POST FOOTING SIZE		
SOIL TYPE	WIND SPEED	≤ 130 MPH
	LATERAL BEARING PRESSURE, PSF	FOOTING DIA. x DEPTH, FT-IN
SAND, SILTY SAND, CLAYEY SAND, SILTY GRAVEL, AND CLAYEY SAND (SW/SP/SM/SC/GM/GC)	150	1'-4"x4'-0"

**FENCE POST FOOTING NOTES:**

- FOOTING SIZES SHOWN IN THE TABLE BELOW ARE BASED ON GENERALIZED SOIL CLASSIFICATION IN ACCORDANCE WITH CHAIN LINK FENCE WIND LOAD GUIDE WLG 2445 BY CHAIN LINK FENCE MANUFACTURER'S INSTITUTE (CLFM), UPDATED JANUARY, 2023.



**FOOTING FOR TERMINAL OR CORNER POST**

**FOOTING FOR GATE POST**

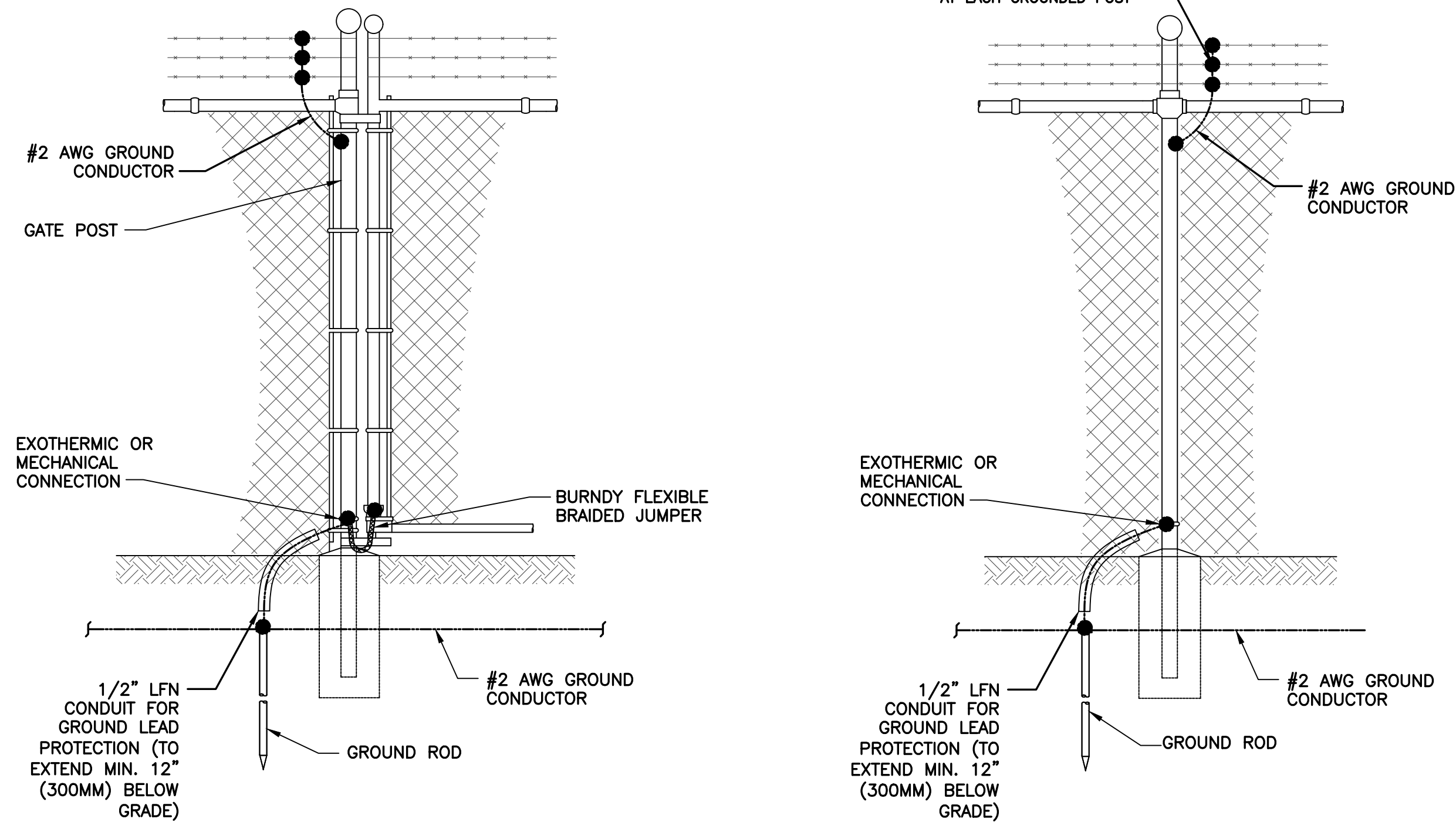
**FOOTING FOR LINE POST**

**FOOTING IN CORED BEDROCK**

**1 POST FOOTINGS**  
S-3 NTS

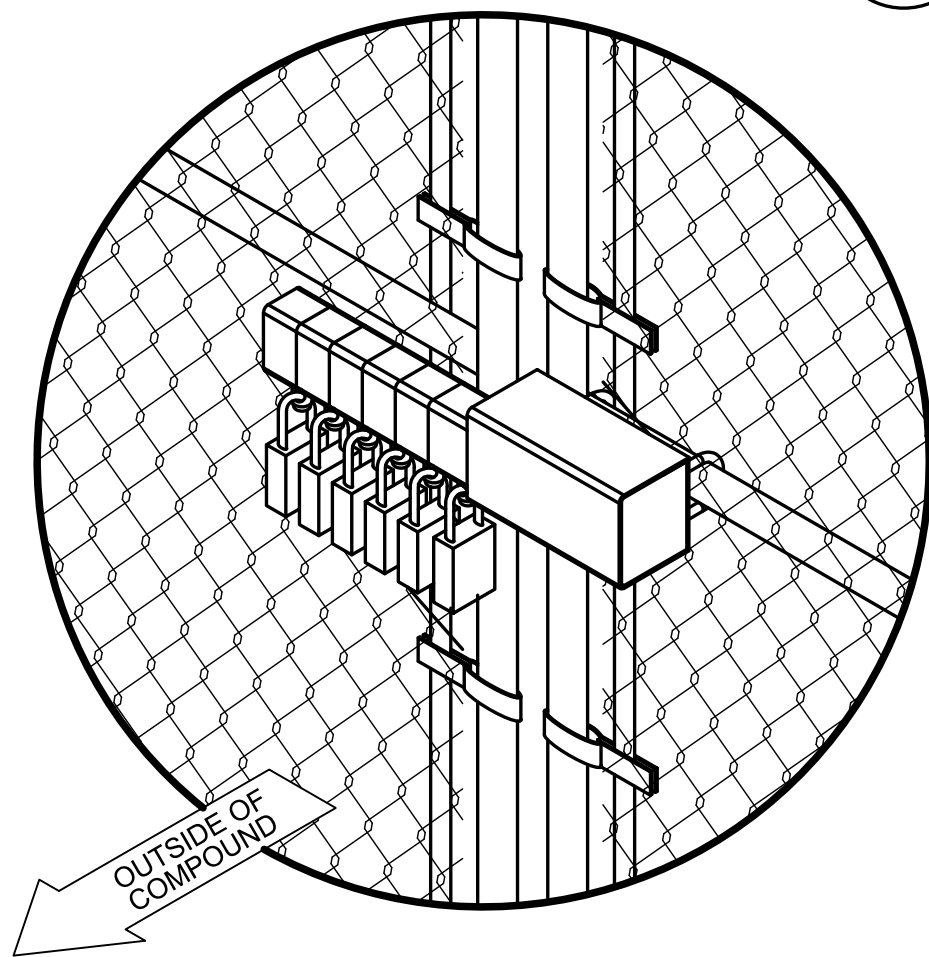
**FENCE GROUNDING NOTES:**

- PROVIDE SEPARATE GROUNDING CONNECTIONS AT ALL CORNER POSTS AND TWO ADDITIONAL POSTS PER SIDE OF THE FENCE.
- PROVIDE GROUNDING ON BOTH SIDES OF ALL PERSONNEL AND VEHICLE GATES.
- FENCE POST SHALL BE CONNECTED TO GROUNDING ROD USING #2 AWG (OR LOCAL EQUIVALENT) TINNED COPPER WIRE THAT IS CLAMPED, BRAZED, OR WELDED.

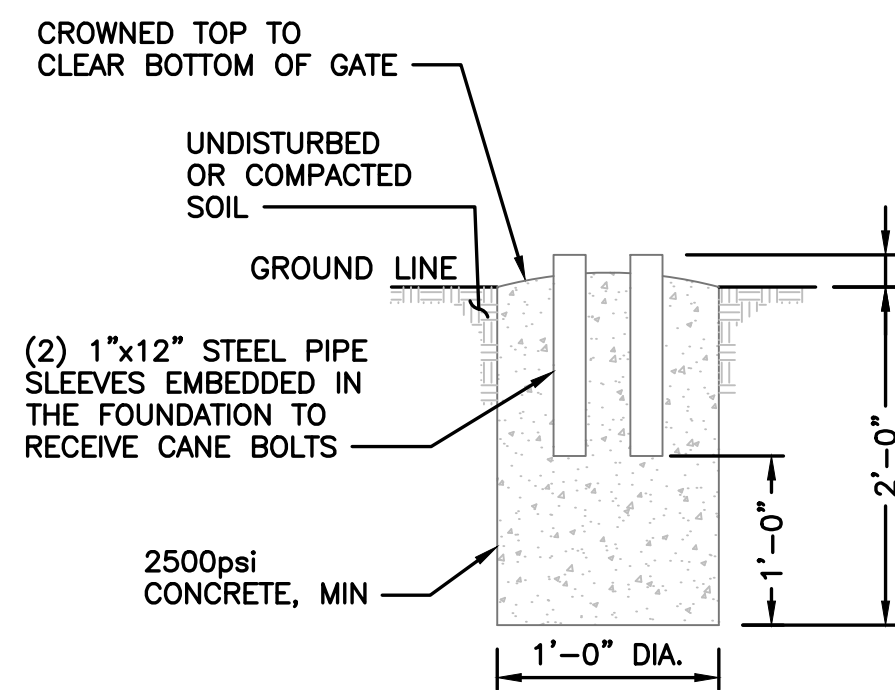


**4 GATE AND GATE POST**  
S-3 NTS

**5 TANGENT POST FENCE**  
S-3 NTS



**2 STYMIE LOCK DETAIL**  
S-3 NTS



**3 DOUBLE CANE BOLT CATCH**  
S-3 NTS

**ENGINEERING FIRM**

**NB+C**  
TOTALLY COMMITTED.

NB+C ENGINEERING SERVICES, LLC.  
120 EASTSHORE DRIVE, SUITE 300  
GLEN ALLEN, VA 23059  
(804) 548-4079

**APPLICANT**

**verticalbridge**  
THE TOWERS, LLC

**CARRIER**

**verizon**

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**PROFESSIONAL STAMP**

COMMONWEALTH OF VIRGINIA  
JOHN A. DAUGHTREY III  
Lic. No. 0402052122  
05/18/26  
PROFESSIONAL ENGINEER

**ENGINEER**

JOHN A. DAUGHTREY III, P.E.  
VIRGINIA PROFESSIONAL ENGINEER  
LICENSE #0402052122

**SHEET TITLE**

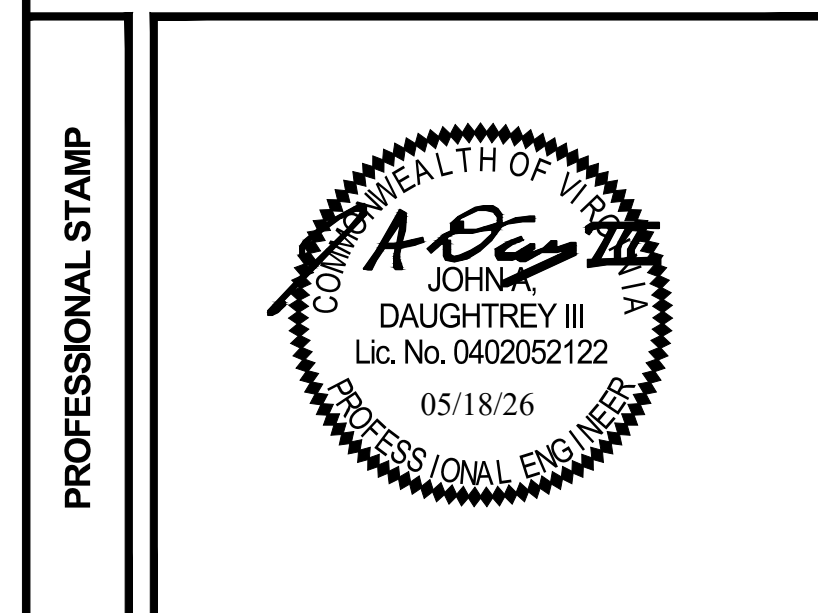
**FENCING DETAILS II**

**SHEET NUMBER**

**S-3**

**DESIGN RECORD**

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1	05/05/26	REVISED FINAL ZDS	JC
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**ENGINEER**

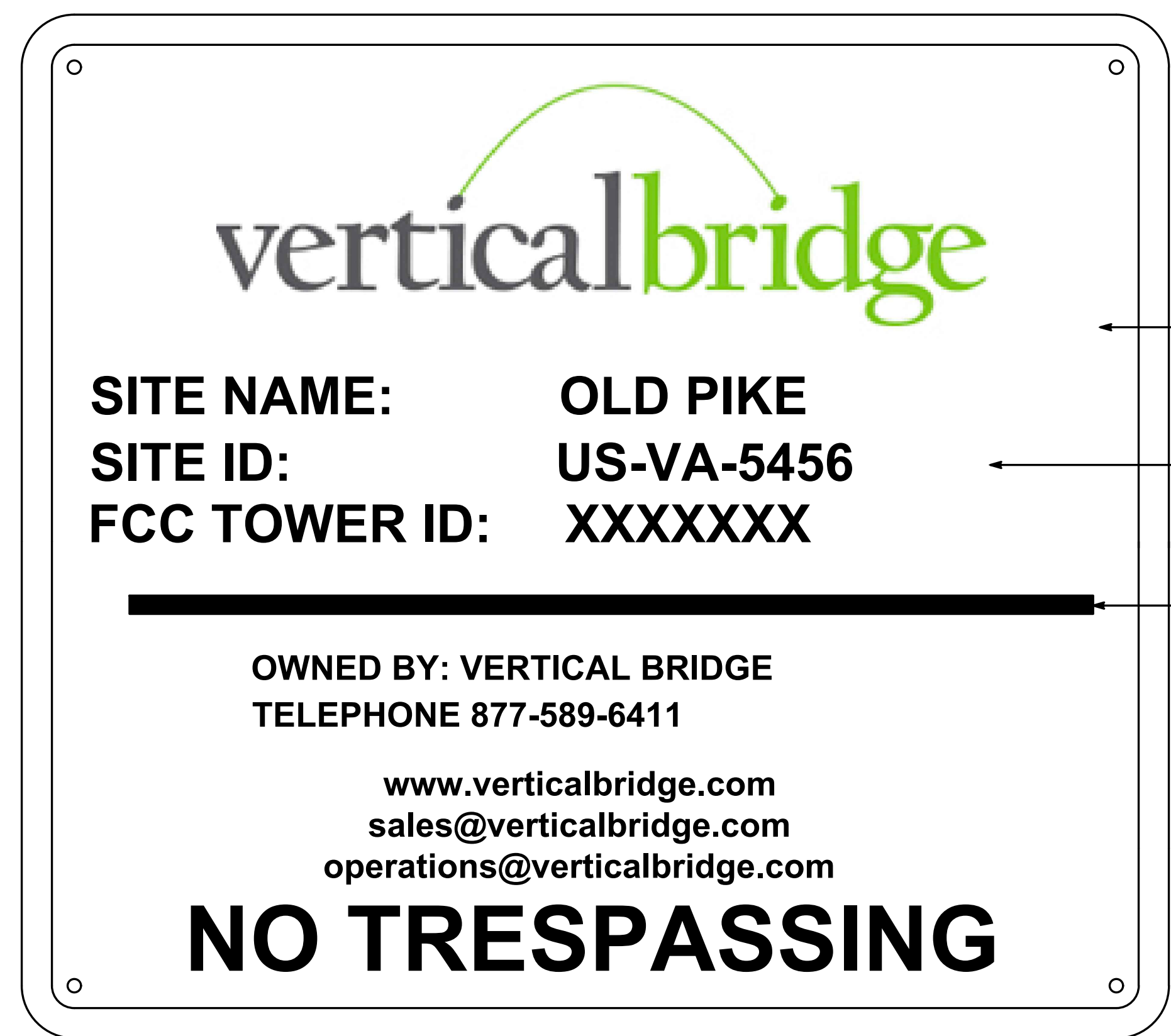
JOHN A. DAUGHTREY III, P.E.  
VIRGINIA PROFESSIONAL ENGINEER  
LICENSE #0402052122

**SHEET TITLE**

**SIGNAGE DETAILS**

**SHEET NUMBER**

**S-4**



QTY: (1) ON RIGHT SIDE OF MAIN COMPOUND GATE, (1) AT ACCESS ROAD GATE, IF EXIST  
CONTRACTOR SHALL VERIFY FCC NUMBER, AND ARCOLA SITE ID PRIOR TO ORDERING SIGNAGE

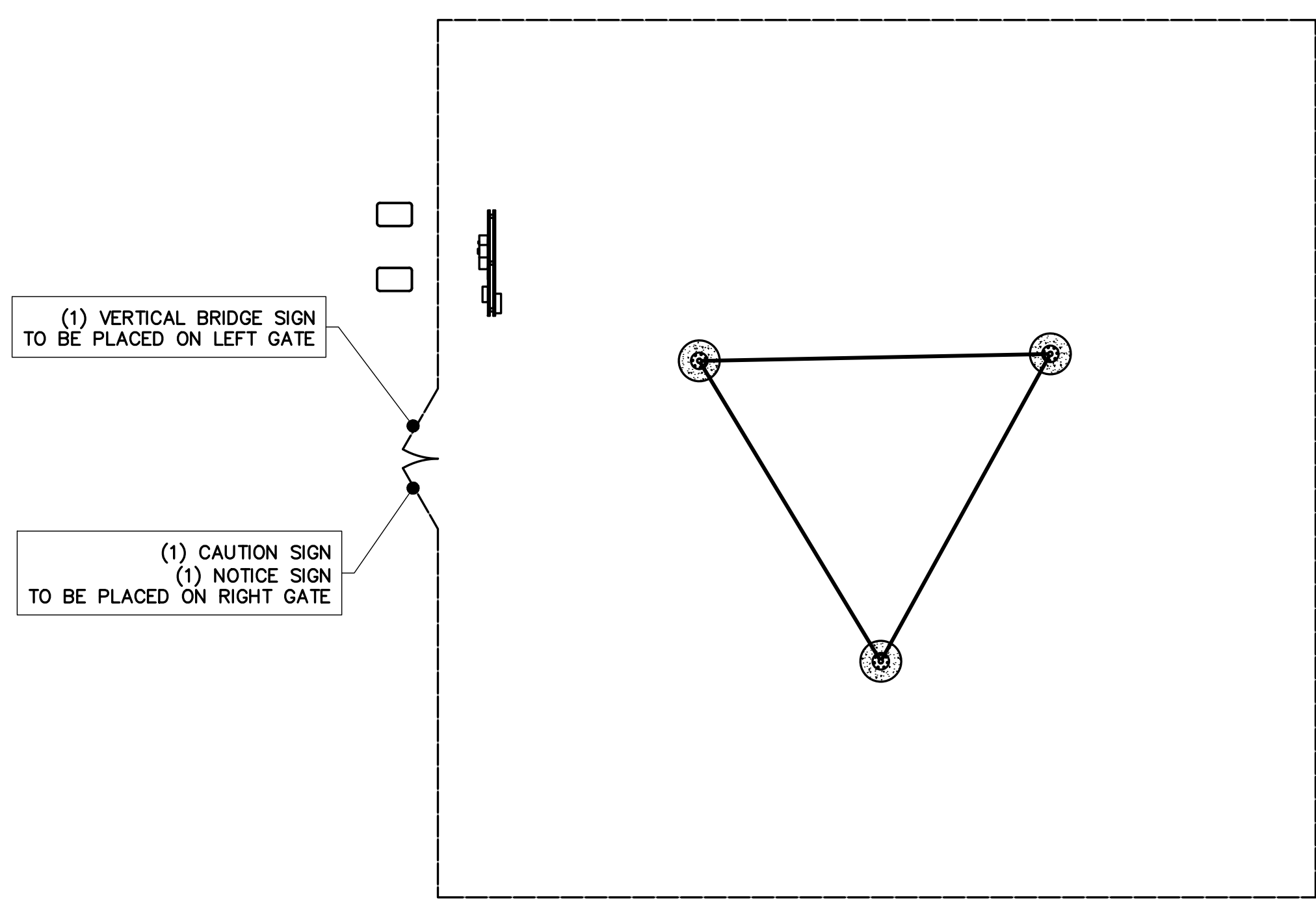
2 VERTICAL BRIDGE - SITE SIGN  
S-4 NTS



3 CAUTION RF SIGN  
S-4 NTS



4 NOTICE RF SIGN  
S-4 NTS



1 SIGN LAYOUT PLAN  
S-4 NTS